



Myers
Planning & Associates

Portland Gateway Precinct Development Plan

Corner of Henty Highway & New Street, Portland
Prepared for Glenelg Shire Council

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Quality assurance

Background Report

Portland Gateway Precinct Development Plan

Corner of Henty Highway & New Street

Prepared for Glenelg Shire Council

Project Number

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01	10 March 2023	Draft Development Plan Background Report
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Acknowledgement of Country



We acknowledge the Traditional Owners of the Country on which the land subject of this report applies, and recognise their continuing connection to the land, water, and culture. We pay our respects to their Elders past, present and emerging.

This Development Plan Background Report is prepared for Glenelg Shire Council.



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1. Introduction

1.1. Development Plan Overview

This Development Plan Background Report has been prepared by Myers Planning & Associates on behalf of Glenelg Shire Council for the Portland Gateway Precinct.

The Development Plan Background Report relates to the future use and development of land generally bordered by Henty Highway, New Street and Browning Street. The site is known as the Portland Gateway Precinct and is envisaged to support a range of commercial uses appropriate to the highway context and prevailing planning framework.

The Development Plan area encompasses approximately 7.59 hectares of land and is within the Commercial 2 Zone at the northern gateway to Portland. The Development Plan area boundary is defined by the extent of the Development Plan Overlay – Schedule 10 of the Glenelg Planning Scheme.

Portland is an important Regional Centre and plays an integral part in supporting the Shire's economy through employment, particularly within the timber and fishing industries which benefit from the location of the natural deep-sea port. It is important for regional centres to grow and become self-sufficient in their provision of services, facilities, and commercial offerings, including those which are envisaged at the Portland Gateway Precinct – such as large-format commercial uses and highway-related service industries.

Having regard to the strategic role of Portland to the broader region, the purpose of the Portland Gateway Precinct Development Plan is to guide future development, within a commercial context, and encourage investment and employment within this part of Portland.

This Development Plan will allow for the holistic and proper planning of the site, including opportunities for targeted stakeholder engagement during the planning process.

1.2. Need for a Development Plan

A development plan is required by Schedule 10 to the Development Plan Overlay (DPO10) which applies to this land.

The Portland Strategic Framework Plan has informed the ordinance for Schedule 10 to the Development Plan Overlay (DPO10) to implement and guide the future land use and development of the subject site. The subject site is located on a significant gateway entrance to the city of Portland, on the confluence of significant transport/freight, industrial and residential land uses. The subject site also comprises the city's largest unoccupied commercial land holding.

It follows that the Portland Gateway Precinct Development Plan is to be prepared in accordance with Schedule 10 to the Development Plan Overlay.

1.3. Vision and Objectives

Providing key objectives and a vision for the Development Plan is imperative as this Development Plan will be the guiding document for future subdivision, land use and development applications for the subject site / development plan area.

The Portland Gateway Precinct comprises several features unique to the area, including:

- Gateway entrance location into Portland
- Access to key arterial roads and linkages, including the Henty Highway, New Street and Browning Street.
- A large vacant area of commercial land holdings in common ownership on a corner site.
- Proximity to existing development infrastructure and services.

These features create an opportunity for a unique commercial precinct which responds to the objectives of Schedule 10 to the Development Plan Overlay. The development plan aims to implement the following vision:

The Portland Gateway Precinct is to be a thriving commercial hub that leverages its prime location at the entrance to Portland, excellent visibility from the surrounding area, and strong connectivity to road networks.

The precinct will be attractive to large-format commercial premises, which are expected to attract a sympathetic range of retail and commercial employment uses.

As the leading large-format commercial precinct in Portland, the precinct will prioritise the operator and customer experience by offering ample accessibility, parking, and loading/unloading facilities.

The precinct's appeal will be further enhanced by attractive built form outcomes, which create an inviting retail frontage and a strong sense of place.

This vision will generally be achieved by:

- Providing for commercial development which is compatible with the characteristics of the Portland Gateway Precinct environs through design and interface treatments, including signage and landscaping.
- The creation of developable areas appropriate for large-format retail and appropriate staging to ensure the coordination of infrastructure and servicing viability of the precinct.
- Providing opportunities for a range of internal road network arrangements to connect to key external road connections/intersections.
- Creating a commercial environment with a strong sense of place through well-coordinated allotments and built form which allows for activation to existing road reserves as well as internal roads.
- The provision of appropriate services and infrastructure to the satisfaction of the relevant authorities.
- Managing stormwater quality and quantity generated from the subject site and the wider catchment and ensuring it does not adversely affect downstream waterways.
- Encouraging the provision of landscape buffers and acoustic treatments to assist with visual softening and noise attenuation of adjoining residential areas, and associated amenity expectations.
- Supporting development applications which cater appropriately to likely car parking generation/demand, associated car parking requirements and provide for appropriate loading and unloading areas.

1.4. Structure of the Development Plan

This Development Plan Background Report comprises a series of plans with explanatory and background text, which illustrate relevant land use, built form, physical and environmental conditions, access/transport, development service infrastructure and other relevant issues, opportunities, and recommendations to guide future land use and development of the subject site.

The content structure is as follows:

- **Site and Context:** an overview of the subject site, development area and immediate locality.
- **Planning and Policy Context:** an overview of relevant Council and regional strategies, the Municipal Planning Strategy, Planning Policy Framework and zones and overlays affecting the development plan area.
- **Response to Schedule 10 to the Development Plan Overlay:** outlines the contents and requirements of Schedule 10 to the Development Plan Overlay (**DPO10**).
- **Site analysis:** details the surrounding land uses, topography, views and vista, areas of environmental and cultural significance and existing infrastructure services.
- **Vision Response:** Provides a high-level concept plan which starts to bring a vision together for what the development plan could look like using the knowledge of the site analysis.

The Development Plan Background Report should also be read in conjunction with **Appendix B** – Infrastructure Services Report.

2. Site and surrounds

2.1. Development plan area and location

Portland is a city situated on the coast, which serves as a regional centre in the southern part of Australia. It is approximately 350 kilometres west of Melbourne and 550 kilometres southeast of Adelaide. The nearest regional city is Warrnambool, which is approximately 100 kilometres east of Portland and can be reached via the Princes Highway.

The specific location being referred to is the Portland Gateway Precinct, which is about 1.8 kilometres north of the Portland Central Business District. The site is situated 250 meters west of Portland Bay. The Development Plan area, also known as 'the Site', is bound by the Henty Highway, New Street, and Browning Street and more narrowly defined by the extent of the Commercial 2 Zone and mapped extent of DPO10. Additionally, the site has frontage to Richardson Street, which runs north to south through Learmonth Street from the neighbouring residential area.

The Site comprises five lots/parcels that vary in size and shape, totalling approximately 7.9 hectares. The land is undulating and slopes generally from the southwest corner to the northeast corner. Vegetation on the site is scarce but there are patches of native trees along the road reserves of Browning Street, New Street, and the Henty Highway.

The Site is located to the southeast of the recently approved Development Plan for the North Portland Industrial Precinct. This Development Plan was jointly prepared by the Victorian Planning Authority and Glenelg Shire Council and includes requirements and guidelines for the development of that industrial precinct for the development of the surrounding industrial precinct.

Refer to Figure 1 (Aerial Plan) and Figure 2 (Portland location).



Figure 1: Aerial Plan (Development Plan Area)



Figure 2: Portland Location (Victorian Settlement Framework Plan)

2.2. Site description and title particulars

Site address	Henty Highway and 68 Richardson Street, Portland
Title details	Lot 1, Lot 2, and Lot 3 on Plan of Subdivision 406253H Lot 1 and Lot 2 on Plan of Subdivision 406254F
Site description	The Site is irregular in shape with an area of 7.9 hectares and a frontage of approximately 217 metres to New Street, 520 metres to Henty Highway and 132 metres to Browning Street.
Landowners	For the purposes of the <i>Planning and Environment Act 1987</i> , at the time of writing, Lord Buddha Pty Ltd are the registered landowners for all parcels within the Development Plan area.
Easements, restrictions, or covenants	<p>The site is not encumbered by any agreements pursuant to Section 173 of the <i>Planning and Environment Act 1987</i> and the <i>Subdivision Act 1988</i>; however, PS406254 contains several covenants, including a restriction on earth from being carried away as well as a restriction on winning or manufacturing of brick tiles or pottery.</p> <p>There are several easements on the plan of subdivision as follows:</p> <p><u>PS406254F</u></p> <p>E1: Telecommunications and drainage easement at 3.5 metres wide, along the western boundary, running north-south.</p> <p><u>PS406254H</u></p> <p>E1: Drainage and sewerage easement 3 meters in width for a length of 47.4 meters along the southern boundary of Lot 3.</p> <p>E2: Drainage and sewerage easement 2 metres in width along the western title boundary of Lot 3.</p>

Refer to **Appendix A** - Certificates of Title.

2.3. Site context and surrounds

The Development Plan area is situated at the northern entrance to Portland, where the Henty Highway intersects with New Street and Lowe Street (leading to Dutton Way and Hanlon Parade). The wide road reserves play a major role in defining the character of the area, as they facilitate efficient vehicular access to and from Portland and provide wide separation between built form areas.

The precinct interfaces with a mix of land uses, with residential neighbourhoods located to the south and east, with industrial areas positioned to the north and west and a small precinct of Commercial 2 Zone located northeast of the Henty Highway/Lowe Street intersection.

The Site's key interfaces are as follows:

North	Beyond the Henty Highway to the north, Lot 1 TP585521 Henty Highway is a vacant industrial parcel of land approximately 2000 square metres in size. A planning permit for an industrial subdivision has been issued for the site and works appear underway to service the development.
East	Beyond the New Street Road Reserve to the east is private land for residential purposes with frontages to New Street and Hanlon Parade.
South	Learmonth Street and Richardson Street which both comprise existing residential development.
West	Browning Street and 120 Learmonth Street which contains industrial land used predominately for warehouse related uses.

Refer to Figure 3 - Context Plan showing the current zoning context.



Figure 3: Context Plan

3. Planning policies and controls

3.1. Municipal Planning Strategy (MPS)

The Municipal Planning Strategy (MPS) sets out local and regional planning policy with a focus on specific areas and issues within Glenelg Shire Council. The Municipal Planning Strategy (MPS) policies relevant to the Development Plan are detailed below.

- **Clause 02.01 – Context** – identifies the geographic and demographic profile of Glenelg Shire as well as providing background information to be used in a planning context. Glenelg Shire had a total population of 20,152 in 2021 (ABS 2021). The Shire is projected to experience slow population growth over the next 25 years. This clause also identifies the Shire's regional strengths, including its rich natural resources, natural deep-sea port with road and rail links, as well as strong primary production industries such as for fishing and timber. Further, this clause nominates that the aluminium smelter at Portland is an important sector of the economic, with retailing being the largest of the Shires service industries.
- **Clause 02.03 – Strategic Directions**
 - o **Clause 02.03-1 – Settlement**
Regional Centre (Portland) – Portland is the Shire's main population, business, and administrative centre.
 - o **Clause 02.03-2 Environmental and landscape values**
Coastal Areas – Portland's coastal areas are a major natural, economic, and environmental resource. The Shire seeks to protect these areas by:
 - Protecting the natural and cultural values of the coast.
 - Managing the use and development of the coast in a sustainable manner.
 - Maintaining locally significant views that contribute to the character of the coast and coastal hinterland region.
 - o **Clause 02.03-3 Environmental risks and amenity**
Bushfire – identifies the interface areas of Portland to be of a bushfire risk. The Shire seeks to manage bushfire risk by:
 - Discouraging development in areas prone to bushfire risk.
 - Supporting planning and development considerations that minimise the impact of fire.
 - o **Clause 02.02-5 Built environment and heritage**
Built environment – Protecting existing heritage values and the presentation of towns from highways and major roads assists in maintaining character. Council seeks to improve the built environment by:
 - Encouraging development at township entrances to be designed and landscaped in a manner that contributes to the appearance and character of the town.
 - Supporting development that respects the existing character of towns.

Heritage – The Shire contains natural and cultural heritage which defines its character and contributes to the attraction of the Shire as a desirable place to live, invest and visit. Council seeks to respond to its heritage by protecting places and areas of natural and cultural heritage.

Clause 02.02-6 Economic development
Employment and industry - The major concentrations of jobs within Portland are in manufacturing, agriculture, forestry and fishing, retail, education and training, accommodation, and food services. The Shire will promote employment and industrial growth by:

- Supporting use and development that increases employment in the Shire particularly in Glenelg's key employment/industry sectors.
- **Clause 02.04 – Glenelg Shire Council Strategic Framework Plans**
The below map shows the Portland Framework Plan within this clause. The subject site is identified as 'development plan area' within the northern extent of the Framework Plan adjoining the Henty Highway (Key Industrial Route):

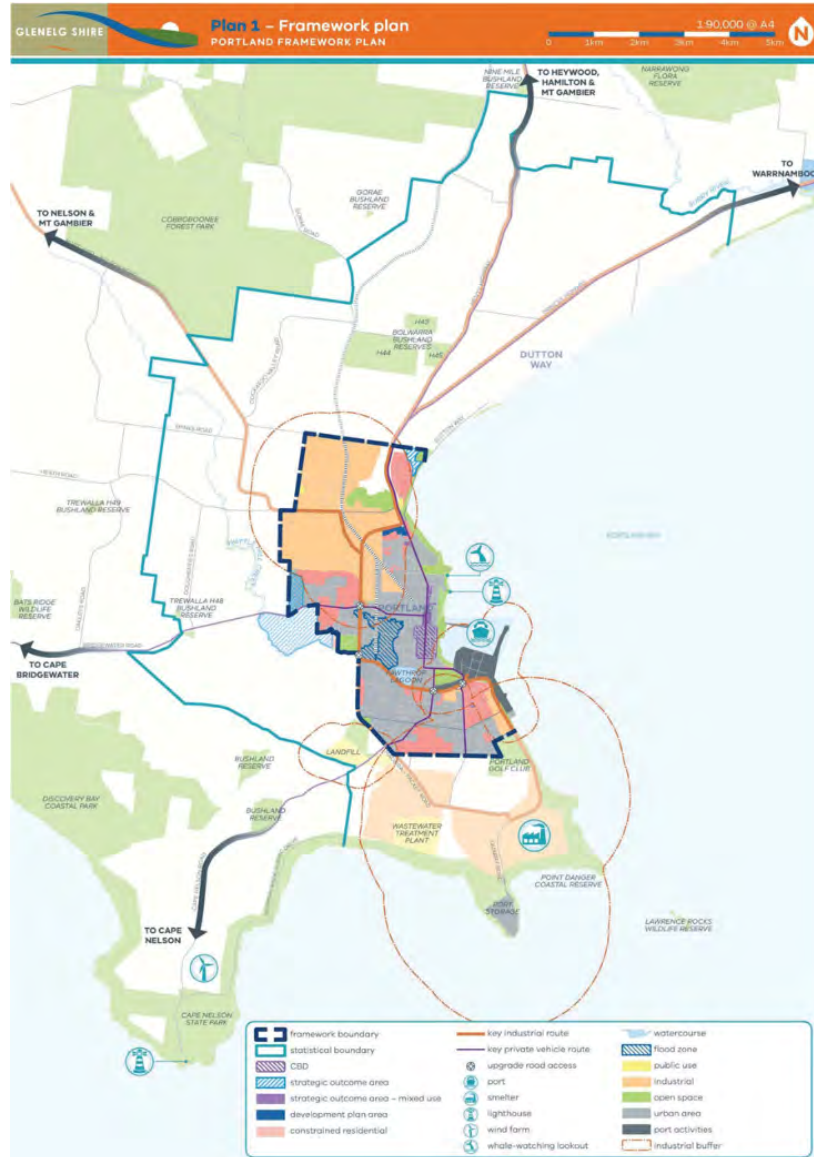


Figure 4: Portland Framework Plan

3.2. Planning Policy Framework (PPF)

To ensure the overarching objectives of planning in Victoria are met, policies contained within the Planning Policy Framework (PPF) must be considered. The PPF clauses of most relevance the Development Plan are set out below.

Settlement

- Clause 11.01-1S – **Settlement** – seeks to facilitate the sustainable growth and development of Victoria and deliver choice and opportunity for all Victorians through a network of settlements.
- Clause 11.01-1R – **Settlement – Great South Coast** – seeks to attract more people to the region.
- Clause 11.03-1S – **Activity Centres** – seeks to encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.
- Clause 11.03-4S – **Coastal Settlement** – seeks to plan for sustainable coastal development.

- Clause 11.03-4L – **Coastal Settlement** – includes strategies to:
 - o Confine development to within existing coastal settlements.
 - o Minimise visual intrusion of development and retain a dominant natural character within 500 metres of the edge of the coast.
- Clause 11.03-6S – **Regional and local places** – seeks to facilitate place-based planning.

Environmental and Landscape Values

- Clause 12.01-1S – **Protection of biodiversity** – seeks to protect and enhance Victoria's biodiversity.
- Clause 12.02-1S – **Protection of the marine and coastal environment** – seeks to protect and enhance the marine and coastal environment.

Environmental Risks and Amenity

- Clause 13.02-1S – **Bushfire planning** – seeks to strengthen the resilience of settlements and communities to bushfire through risk-based planning that prioritises the protection of human life.
- Clause 13.05-1S – **Noise management** – seeks to assist the management of noise effects on sensitive land uses.
- Clause 13.07-1S – **Land use compatibility** – seeks to protect community amenity, human health and safety while facilitating appropriate commercial, industrial, infrastructure or other uses with potential adverse off-site impacts.

Built Environment and Heritage

- Clause 15.01-1S – **Urban Design** – seeks to create urban environments that are safe, healthy, functional, and enjoyable and that contribute to a sense of place and cultural identity.
- Clause 15.01-1L-02 – **Urban Design Portland** – seeks to maintain and enhance the existing urban character, ensuring the natural environment and landscape remain an important part of the urban form and to improve the presentation of the settlement for visitors and residents alike.
- Clause 15.01-2S – **Building design** – seeks to achieve building design and siting outcomes that contribute positively to the local context, enhance the public realm and support environmentally sustainable development.
- Clause 15.03-2S – **Aboriginal cultural heritage** – seeks to ensure the protection and conservation of places of Aboriginal cultural heritage significance.

Economic Development

- Clause 17.01-1S – **Diversified economy** – seeks to strengthen and diversify the economy.
- Clause 17.01-2S – **Innovation and research** – seeks to create opportunities for innovation and the knowledge economy within existing and emerging industries, research, and education.
- Clause 17.02-1S – **Business** – seeks to encourage development that meets the community's needs for retail, entertainment, office, and other commercial services.

Transport

- Clause 18.01-1S – **Land use and transport integration** – seeks to facilitate access to social, cultural, and economic opportunities by effectively integrating land use and transport.
- Clause 18.01-2S – **Transport System** – seeks to facilitate the efficient, coordinated and reliable movement of people and goods by developing an integrated and efficient transport system.
- Clause 18.02-4S – **Roads** – seeks to facilitate an efficient and safe road network that integrates all movement networks and makes best use of existing infrastructure.

Infrastructure

- Clause 19.03-2S – **Infrastructure design and provision** – seek to provide timely, efficient and cost-effective development infrastructure that meets the needs of the community.
- Clause 19.03-2L – **Infrastructure design and provision** – includes strategies to:
 - o Improve the infrastructure capacity of Portland and other towns to provide for further population and industrial growth.
 - o Provide a consistent approach to the design and construction of infrastructure across the municipality.
- Clause 19.03-3S – **Integrated water management** – seeks to sustainably manage water supply and demand, water resources, wastewater, drainage and stormwater through an integrated water management approach.

3.3. Zones

3.3.1. Commercial 2 Zone

The Development Plan area is located within the Commercial 2 Zone (C2Z). The purposes of the Commercial 2 Zone included:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To encourage commercial areas for offices, appropriate manufacturing and industries, bulky goods retailing, other retail uses, and associated business and commercial services.
- To ensure that uses do not affect the safety and amenity of adjacent, more sensitive uses.

Clause 34.02 includes provisions which relate to land use, subdivision and buildings and works. Signage within the Commercial 2 Zone is in Category 1 with reference to Clause 52.05.

Application requirements and decision guidelines are found at Clause 34.02-5 and Clause 34.02-7, respectively.

3.4. Overlays

3.4.1. Development Plan Overlay

The Development Plan area is located within the Development Plan Overlay – Schedule 10 (DPO10).

The purposes of the Development Plan Overlay include the following:

- To implement the Municipal Planning Strategy and the Planning Policy Framework.
- To identify areas which require the form and conditions of future use and development to be shown on a development plan before a permit can be granted to use or develop the land.
- To exempt an application from notice and review if a development plan has been prepared to the satisfaction of the responsible authority.

Clause 43.04-1 refers the reader to any schedule to the overlay for specific objectives.

Clause 43.04-4 outlines the requirements for the preparation of the development plan, including that the development plan must describe:

- The land to which the plan applies.
- The proposed use and development of each part of the land.
- Any other requirements specified for the plan in a schedule to this overlay.

Refer to Section 4.0 of this Development Plan Report for details of the Development Plan Overlay.

3.5. Other planning documents and strategies

3.5.1. Portland Strategic Framework Plan

The Portland Strategic Framework Plan 2020 is a comprehensive plan developed by Glenelg Shire Council in Victoria, to guide the future development of the town of Portland. The plan identifies key strategic directions and priorities for the town, based on community input and feedback.

Some of the key priorities identified in the plan include improving the quality of public spaces, promoting sustainable development, enhancing community engagement and participation, and attracting investment and new businesses to the area. The plan also aims to preserve and enhance the natural environment and cultural heritage of the area, and to promote tourism and marketing initiatives to increase visitor numbers and boost the local economy.

Of relevance to the Site, the Framework Plan nominates the subject site as a 'development plan area'.

The vision of the Framework Plan includes:

Portland will be a regional centre that is a desirable place to live, with high residential amenity and diversity of housing stock to meet the needs of a changing population. Land is to be provided for long term residential, commercial, industrial and port - related needs, where incompatible land uses are adequately separated from each other, and their potential impacts are appropriately managed.

The Framework Plan provides for four key outcomes,

- Outcome 1: A Great Place to Live
- Outcome 2: A Great Place to Work
- Outcome 3: A Connected Portland
- Outcome 4: Celebrating Portland's Heritage, Landscape and Natural Environment

Within each of these strategic outcomes, there are specific goals and actions that the Council will undertake to achieve its vision for the town.

Relevant strategic outcomes, objectives and actions provided in the Framework Plan which have been considered in the preparation of the Development Plan, are outlined below:

Outcome 2: A Great Place to Work

Objective O2.3: To provide more commercial opportunities and an enhanced retail environment to service both the local community and increased tourism demand.

Action A2.3.3: Identify opportunities to redevelop the vacant site on the southwest corner of the Henty Highway and New Street to avoid development in an ad hoc manner under the Commercial 2 Zone. There is an opportunity to work with the landowner to prepare a master plan for the site to facilitate development in an appropriate manner.

Outcome 3: A Connected Portland

Objective O3.01: To plan for future transport capacity and demand by improving the efficiency and safety of moving freight.

Action A3.1.6: Close local road access to the 'Ring Road' or reduce access from local roads to left turn only access.

Outcome 4: Heritage, Landscape and Natural Environment

Objective O4.3: To protect Portland's environmental character and conservation value.

Action A4.3.1: Develop a landscape and urban design strategy to improve entrance points to the urban area.

The entry to Portland is referenced several times within the Framework Plan, noting that the outer entrance to the urban area commences at the junction of the Henty Highway and New Street. New Street forms the 'arrival zone' into the central built-up urban area. The Framework Plan states that in some areas the New Street streetscape needs improvement in terms of the quality of buildings, landscaping design and co-ordination of signage.



Figure 5: Portland Framework Plan - Outcome 3 A Connected Portland

3.5.2. Glenelg Strategic Futures Plan – Infrastructure Strategy

The Glenelg Strategic Futures Plan provides a 20-year plan for growth within the shire and involved an investigation into the existing and future assets and engineering infrastructure services to support the strategic plan.

The Infrastructure Strategy provides for high level comments with respect to the capacity of existing infrastructure across Portland and the associated recommendations for remedial action.

With respect to the Development Plan area, this Infrastructure Strategy nominated (in 2009) that the area had an identified drainage issue – with any future development in this area to consider a suitable drainage outfall strategy. The report identifies that there is a lack of trunk infrastructure in fragmented industrial areas as well as poor existing coastal drainage along Hanlon Parade.

It is noted that new infrastructure has been implemented within proximity to the subject site since the 2009 Infrastructure Strategy, largely addressing the above issues of stormwater capacity for the North Portland precinct and the subject site.

This Development Plan Report provides a description of the existing infrastructure within proximity to the site as well recently proposed infrastructure service connections (i.e., approved and not yet built).

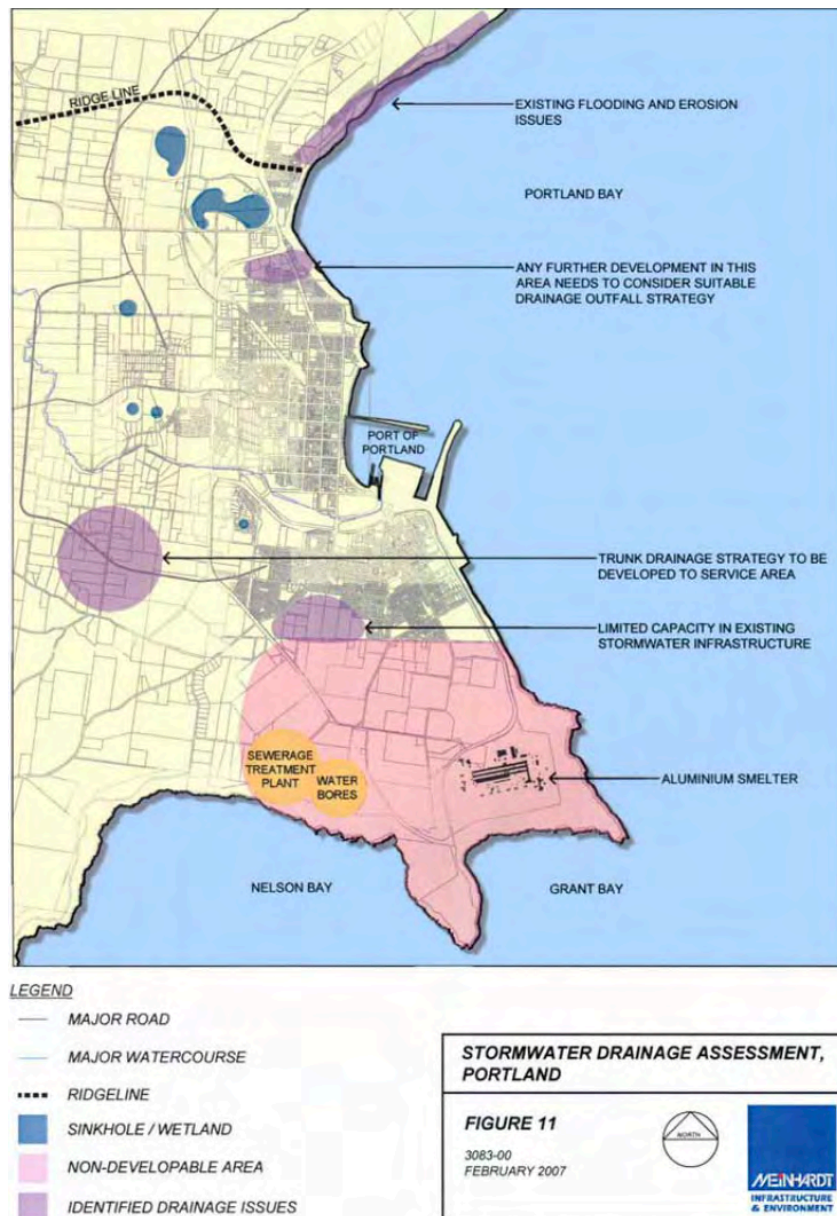


Figure 6: Stormwater Drainage Assessment (Infrastructure Strategy)

3.5.3. Glenelg Shire 2040 Community Plan

The Glenelg Shire 2040 Community Plan captures a long-term vision and sets goals to create a positive future for Glenelg Shire. The plan was supported by members across several Community Reference Group, and it is to be used as a document to guide future decisions of Council and the actions of the community. The Community Plan identified six themes and related priorities,

1. Our Natural Environment
2. Our Education, Employment and Industry
3. Our Health and Wellbeing
4. Our Lifestyle, Neighbourhood and Culture
5. Our Access, Transport and Technology
6. Our Voice and Action

Within these themes, the key Community priorities identified within the Plan most relevant to the Development Plan proposal include:

- Ensure our roads are safe for our community, industry, and tourists.
- Encourage diverse employment opportunities.

- Support our local businesses to increase vitality and prosperity.
- Explore alternative and innovative business practices.
- Invest in our appearance and protect our history.

3.5.4. Green Triangle Freight Action Plan

The Green Triangle Freight Action Plan Update 2016 is a report that outlines the progress of the Green Triangle region in South Australia and Victoria, Australia in implementing the Green Triangle Freight Action Plan (GTFAP).

The GTFAP is a long-term strategic plan aimed at improving the efficiency and safety of freight transport in the Green Triangle region. The report provides an update on the implementation of the GTFAP since its launch.

The update highlights the progress made towards achieving the GTFAP's objectives, while acknowledging the challenges that remain, particularly in relation to increasing investment in infrastructure and improving freight access to key ports.

Further of relevance to the subject site, the Action Plan discusses the Henty Highway as a critical freight route in the Green Triangle region. The Henty Highway is an important link between the major ports of Portland and Geelong, and it serves as a major corridor for transporting goods such as timber, dairy products, and grain.

The report acknowledges that the condition and capacity of the Henty Highway are important factors in the efficiency and safety of freight transport in the region. The plan outlines several initiatives to improve the highway, such as road widening and intersection upgrades, to better accommodate heavy vehicles and improve traffic flow.

Additionally, the report highlights the importance of alternative transport modes such as rail to reduce the pressure on the Henty Highway and improve the overall sustainability of freight transport in the region. The plan encourages the development of rail infrastructure and the promotion of intermodal transport to reduce road congestion and emissions.

To improve access to the Henty Highway, the report outlines several initiatives aimed at improving the condition and capacity of roads that connect to the highway. These initiatives include road widening, bridge upgrades, and improvements to intersections and other key infrastructure.

The plan also recognises the importance of safety in providing access to the Henty Highway. The report highlights the need to address safety issues such as road signage, speed limits, and other factors that can impact the safety of freight transport in the region.

Referencing this section of the Henty Highway as the Portland Ring Road, the GTFAP nominates that there are to be future improvements to the operation and safety of several intersections due to the increased introduction of next generation High Productivity Freight Vehicles. Within Table 4 of the GTFAP, there is specific mention of an intersection improvement at Henty Highway and New Street. Any design for access to the development plan area will need to consider the impacts of an intersection upgrade.

In addition to physical infrastructure, the report also emphasises the importance of collaboration between industry, government, and community stakeholders to address access issues related to the Henty Highway. The plan encourages stakeholders to work together to identify and address issues related to access to the highway and to promote sustainable and efficient freight movements in the region.

3.5.5. Council Plan 2021 – 2025

The 2021-2025 Council Plan links to the 2040 Community Vision, outlining a four-year strategy, financial plan and budget, building upon the community identified goals and commitments. The plan is to be used to find tangible ways in which Council and work towards achieving the community's vision.

The plan includes the below focus areas as summarised:

1. Community: The council aims to build a strong, inclusive, and connected community through engagement, support, and investment in local infrastructure and services.
2. Environment: The council aims to protect and enhance the natural environment, reduce waste, and promote sustainable practices.
3. Economy: The council aims to support economic growth, job creation, and innovation, particularly in tourism, agriculture, and small business.
4. Governance: The council aims to provide transparent, accountable, and responsive governance, as well as efficient and effective delivery of services.

5. Infrastructure: The council aims to maintain and improve infrastructure, including roads, buildings, and public spaces, to meet the needs of the community.
6. Culture: The council aims to celebrate and promote the cultural diversity and heritage of the shire through events, programs, and partnerships.
7. The plan also includes specific actions and targets for each focus area, as well as a monitoring and reporting framework to track progress towards achieving the plan's goals.

This plan supersedes the 2018 – 2021 plan which is identified within the Glenelg Planning Scheme at Clause 02.02 – Vision.

3.5.6. Plan Melbourne 2017 – 2050

Plan Melbourne seeks to provide for a long-term planning strategy for Melbourne through to 2050 to support growth in population and employment. The document is broken up into seven (7) outcomes. Most of the documentation relates to Greater Melbourne and its metropolitan areas; however, Outcome 7 is the most relevant to regional Victoria and the development Plan area within Portland.

Outcome 7

Regional Victoria is productive, sustainable and supports jobs and economic growth.

Direction 7.1:

Invest in regional Victoria to support housing and economic growth.

Policy 7.1.1:

Stimulate employment and growth in regional cities.

3.5.7. Great South Coast Regional Growth Plan

The Great South Coast Regional Growth Plan 2014 is a strategy document that outlines a vision for the economic and social development of the Great South Coast region of Victoria, Australia. The plan was developed by the Victorian State Government in consultation with local stakeholders and communities.

The plan sets out several goals and objectives for the region, including the creation of new jobs, improved infrastructure, and increased investment in the region. It identifies several priority areas for development, including the expansion of key industries such as agriculture, tourism, and education.

The plan also emphasises the importance of collaboration between government, industry, and community stakeholders in achieving these goals. It outlines several actions and initiatives that will be taken to support regional growth, including the establishment of a Great South Coast Economic Development Board and the development of a Regional Investment Prospectus.

The Great South Coast region benefits from its existing settlement pattern by supporting growth in Warrnambool, Portland and Hamilton, which provides services, housing and jobs throughout the region. Portland will continue to attract retail, commercial and higher order services based on regional needs and demand as well as population.

13.3 Liveability

Land use policies, strategies and actions

Encourage infrastructure that enhances the liveability and attractiveness of the region to residents and commercial activities.

13.9 Regional Centres

Portland Regional Centre

Portland has a major role as a service centre for the network of settlements that surround the town and are supported by it. In particular, Portland and Heywood are expected to develop stronger economic and social connections over the period of this plan.

Land use policies, strategies and actions

Investigate Percy Street, immediately north of Henty Street, for potential CBD expansion and provide for further expansion and development around the corner of the Henty Highway and New Street.

4. Schedule 10 to the Development Plan Overlay

4.1. Overview

Schedule 10 to the Development Plan Overlay is shown on the planning scheme map as DPO10 and is known as the **Portland Gateway Precinct**. The overlay was introduced by C106gelg on 20 January 2023 which implemented relevant recommendations of the Glenelg Shire Planning Scheme Review 2018, Rural Land Strategy 2019, Portland Strategic Framework Plan 2020, Portland Rural Living Assessment 2020, and Glenelg Sustainable Settlement Strategy 2012, and corrects mapping and ordinance anomalies.

4.2. Objectives of the Development Plan

The objectives of the Portland Gateway Precinct Development Plan Overlay (DPO) include the following:

- To create an identifiable character and visual quality for the precinct.
- To create an attractive built form entrance into Portland.
- To take advantage of the entrance location into the Portland CBD via New Street and the bypass/ring road via the Henty Highway.
- To facilitate high profile and large-scale commercial development.

4.3. Requirements before a permit is granted

None specified.

4.4. Conditions and requirements for permits

None specified.

4.5. Requirements for development plan

A development plan must include the following requirements:

- Details of the staging of the development.
- Where relevant, appropriate arrangements for the provision and funding of infrastructure.
- The drainage of the land.
- Site topography having specific regard to future built form.
- The future road network, having specific regard for both vehicle and pedestrian access through and around the site.
- Overall landscaping for the precinct.
- Details of how potential offsite impacts such as odour, dust and noise will be minimised on the amenity of surrounding sensitive uses.
- Urban design features of the overall precinct, specifically streetscape character, place making and identity, and built form character.
- Where subdivision is proposed, the proposed lot layout including larger lots that facilitate large scale commercial development.
- Commercial allotment design including, but not limited to, the location and scale of signs, sustainable building design, water sensitive urban design, landscaped frontage and interfaces with the residential area, site setbacks, and open space interface.

5. Site analysis

5.1. Tenure and easements

Landholdings

The development plan area comprises five (5) title parcels within the same ownership at the time of writing. The total area of the landholdings is approximately 7.9 hectares (see Figure 7).

The certificates of title all present an address of Henty Highway, Portland, except for Lot 2 Plan of Subdivision 406254F which is known as 68 Richardson Street, Portland.

The largest of the landholdings is Lot 3 on Plan of Subdivision 406253H which is 2.119 hectares in size, whilst the smallest of the landholdings is Lot 1 Plan of Subdivision 406254F is 9743 square metres in size.

Lot 1, Lot 2 and Lot 3 of Plan of Subdivision 406253H all present a frontage to Henty Highway, spanning a total of approximately 520 metres. The western end contains an allotment which is on the corner of Henty Highway and Browning Street.

Lot 1 and Lot 2 of Plan of Subdivision 406254F have primary frontage to New Street of 217 metres, with other frontages to Richardson Street, Cope Street and Thomas Street (not constructed).

All landholdings are vacant of any buildings and structures.

Easements

The site is not encumbered by any restrictions or covenants pursuant to Section 173 of the Planning and Environment Act 1987 and the Subdivision Act 1988; however, there are several easements on the plan of subdivision as follows:

PS406254F

E1: Telecommunications and drainage easement at 3.5 metres wide, along the western boundary, running north-south.

PS406254H

E1: Drainage and sewerage easement 3 meters in width for a length of 47.4 meters along the southern boundary of Lot 3.

E2: Drainage and sewerage easement 2 metres in width along the western title boundary of Lot 3.

The existing easements provide opportunity for reuse and to provide for key trunk infrastructure for the development plan area and to adequately service the broader catchment for services.

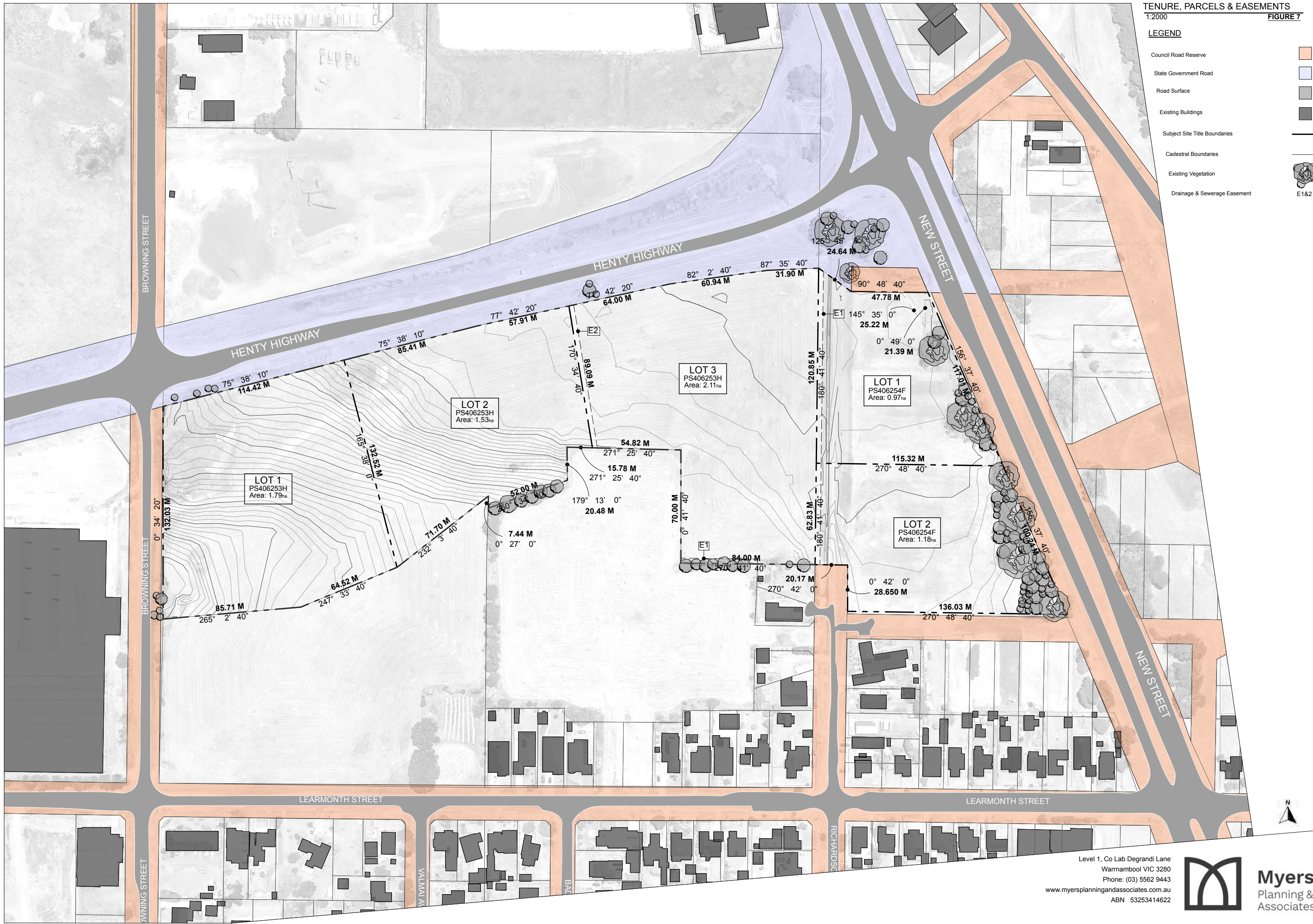
Refer to Figure 7 for the existing property ownership and easements plan.

Key design considerations:

- Enable individual landowner opportunities (if applicable) to develop independently of each other without compromising the development plan.
- Encourage collaboration and contribution with adjoining landowners where easements may be required.

LEGEND

- Council Road Reserve
 - State Government Road
 - Road Surface
 - Existing Buildings
 - Subject Site Title Boundaries
 - Cadastral Boundaries
 - Existing Vegetation
 - Drainage & Sewerage Easement
- E1&2



5.2. Development services

Water

Wannon Water is responsible for providing water supply reticulation facilities for the development. Existing water mains are situated on the west of the site on Browning Street with a 225-millimetre main, while Learmonth Street and Richardson Streets have a 100-millimetre water line, providing services to neighbouring properties and hydrant fill points.

It is anticipated that any new main branch line will be connected to the existing water main infrastructure, running across the site to enable individual branches to service each of the lots with a metered connection.

Gas

The responsibility of providing new gas infrastructure for the development lies with SP Ausnet. Information on the existing gas infrastructure in the vicinity has been provided, which reveals that gas infrastructure is already present in Learmonth Street and Richardson Street.

To provide gas supply to any future proposed lots, the existing gas infrastructure will need to be extended to these connection points.

Electricity

Powercor is the service provider in charge of distributing electricity assets in the region. The subject site located in Browning Street, Henty Highway, and New Street has limited existing power infrastructure. Therefore, it is anticipated that a new supply distribution substation will be necessary for any further development on the site.

Current standards dictate that for both residential and industrial development, new underground power supply must be installed with pits located on the property boundary to connect to each of the lots. Powercor will be responsible for constructing the power main to the pits, which will be at the developer's expense.

Sewer

Wannon Water is responsible for providing sewerage facilities for any development works in Portland. Upon investigation, a 225-millimeter diameter pressure sewer main was found on the northern side of Henty Highway, running east past the entire site and operated by Wannon Water. Additionally, a 150-millimeter gravity sewer line has been identified within Learmonth Street to the south of the site, servicing existing dwellings fronting Learmonth Street.

For a proposed 21-lot subdivision at 56 Learmonth Street, there is a proposed access and minor extension to the existing 150mm sewer gravity line. The proposed sewer design for this subdivision outlines the construction of a new 150-millimeter sewer line that traverses to the north through the site, bores under Henty Highway, and connects to the higher-pressure sewer main north of Henty Highway. This new sewer line will serve the balance of the lots in the subdivision.

For further development within the subject site, the proposed main sewer trunk line to traverse the site is expected to provide sewer services for individual connections.

See **Appendix B** – Infrastructure Services Report for a detailed analysis of utilities and development services.

Refer to Figure 8 for details.

Key design considerations:

- Delivery of efficient infrastructure services through utilising existing service points and road reserves, planned easements and topography where advantageous for reticulated services.
- Support the undergrounding of all infrastructure servicing where practicable.
- Co-locate trunk infrastructure easements, where practicable, to make efficient use of encumbered land.

Guidelines for planning permit applications

- An Infrastructure Services Report comprising a strategy as to how the proposed subdivision or development will be serviced with utilities.

LEGEND

-



5.3. Land uses

The area designated for the Portland Gateway Precinct Development Plan is situated about 1.8 kilometres north of the Portland Central Business District. The land uses in the surrounding areas reflect the settlement pattern of Portland and its geographic location, particularly with regards to Portland Bay and its Port.

North

Towards the north lies a vacant industrial land spanning roughly 2000 square meters, situated on the northern side of the Henty Highway. There is a proposed industrial subdivision on this land, and it has been holistically planned through the Portland North Industrial Precinct Development Plan, authored by the Victorian Planning Authority and Glenelg Shire Council. Beyond this land to the north and northeast area areas comprising a small concrete batching plant and sawmills, which comprise uses with a nominated separation distance as per Clause 53.10 Uses with adverse amenity potential.

South

The area to the south of the Development Plan area is occupied by existing residential development mainly on Learmonth Street and Richardson Street, consisting of detached housing and single-story buildings with open garden settings. Places of worship are also present on Learmonth Street.

East

To the east is New Street, beyond which lie additional existing residential areas before reaching Hanlon Parade and Portland Bay. Many of these residential allotments remain vacant due to infrastructure servicing issues. New Street includes two lanes in each direction separated by a median strip.

West

Towards the west is an existing industrial estate, with the nearest site on Browning Street having a large shed that is presently utilised for agricultural fertiliser product storage and distribution. Land further west still contains industrial land which is part of the Portland North Industrial Precinct.

Refer to Figure 9 for the surround land uses and zoning.

Key design considerations:

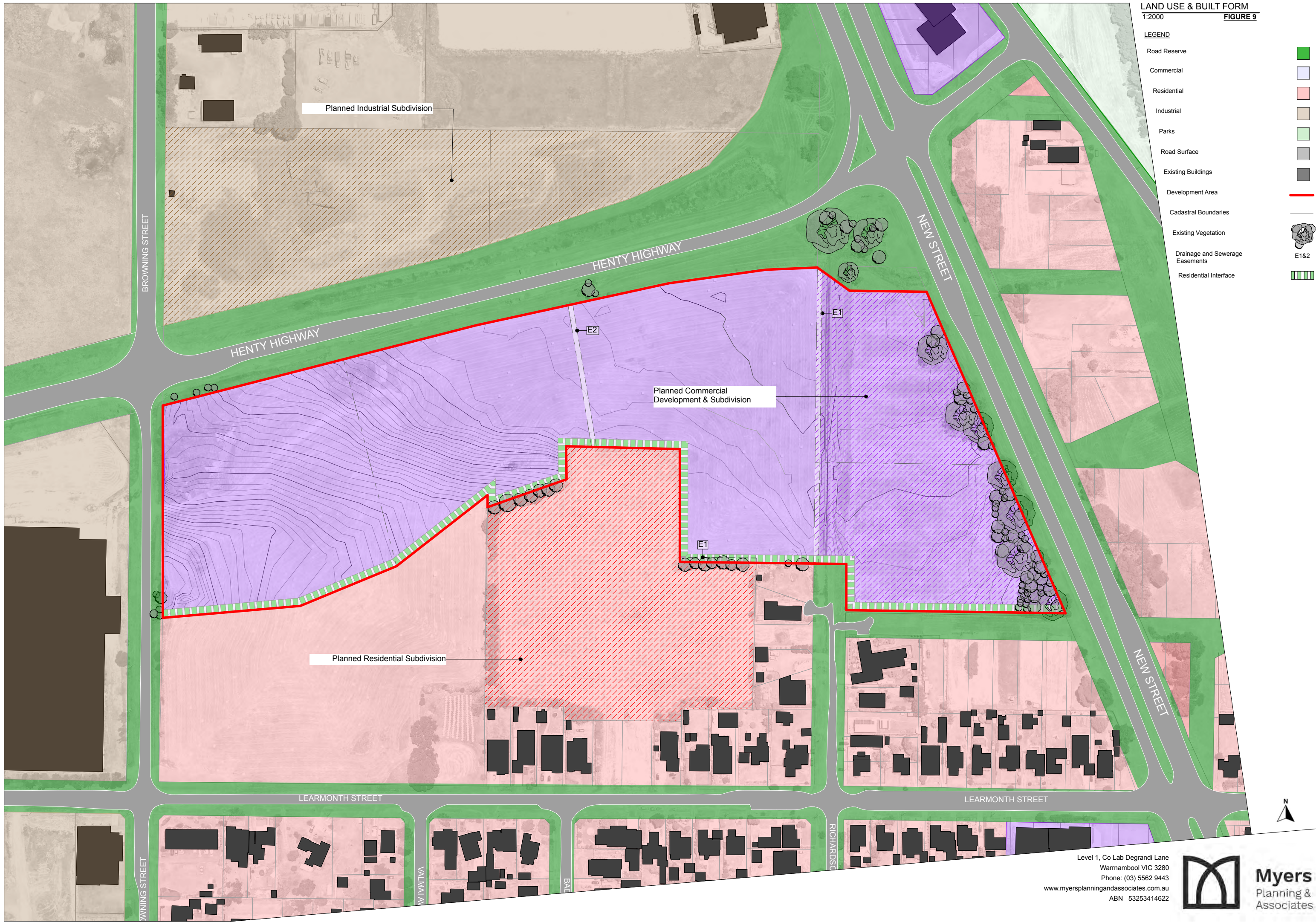
- Facilitate commercial development on the land and ensure an appropriate interface with surrounding land uses.
- Discourage sensitive uses within identified industrial buffers.
- Carefully consider noise attenuation measures to the southern residential interface through setbacks, landscaping and acoustic treatments.
- Provide for road and pedestrian links, where practicable, to surrounding road reserves.
- Provide for entry treatments (landscaping, feature fencing or signage) at key gateway entries to the development plan area.

Guidelines for planning permit applications

- Applications for development must be accompanied by a Landscape Plan which provides for a softened interface to road frontages and allows for an appropriate buffer to the adjoining residential area (south).
- Applications for development must demonstrate through an Acoustic Report that appropriate treatment can be provided to the southern boundary adjoining the residential area.

LEGEND

- Road Reserve
 - Commercial
 - Residential
 - Industrial
 - Parks
 - Road Surface
 - Existing Buildings
 - Development Area
 - Cadastral Boundaries
 - Existing Vegetation
 - Drainage and Sewerage Easements
 - Residential Interface
- E1&2



5.4. Topography, Views and vegetation

Topography

The site exhibits varying topography across its eastern and western portions. The eastern section of the site is mostly flat, and gradually descends towards the existing residential area to the south. Meanwhile, the western section features moderate slopes descending towards the centre of the site. Overall, the land slopes from the southwest to the northeast. In the southwestern corner of the site, there is a peak adjacent to Browning Street, creating a crest in the road reserve before descending towards the intersection with Henty Highway. The highest point on the site has an elevation of approximately 32.3 metres Australian Height Datum (AHD), while the lowest point is situated at the northeast with an elevation of 21.86 metres AHD.

Views

The Portland Gateway Precinct is situated at the intersection of Henty Highway and New Street, making it highly visible to the primary vehicle entry and exit of Portland. However, the development plan area is limited in terms of the key sight lines that can be seen from the site. The key views are primarily looking towards the site and along the surrounding road network, such as north-south along Henty Highway-New Street, west along Henty Highway and north-south along Browning Street. The Site does not include any key internal views or looking outward from the site.

Vegetation

Land within the development plan area has been subject to significant modification through clearing and general maintenance of the land. The vacant parcels are now dominated by exotic grasses vegetation except for the eastern titles, which contains 39 trees located along the frontage to New Street. It is likely that most of the Victorian Native and Indigenous species within the Site have been planted as an old boundary planting based on the pattern of vegetation.

Vegetation which is located within the immediately adjoining road reserve or on private property has also been mapped for the purposes of the Development Plan context.

The native vegetation is not in an area mapped as an endangered Ecological Vegetation Class, sensitive wetland or coastal area. The Ecological Vegetation Class for the area is EVC: Herb-rich Foothill Forest (EVC Number 23) within the Victorian Volcanic Plain Bioregion. It is part of the Dry Forests Group, with the conservation status of this region as 'vulnerable'.

The Strategic Biodiversity Score varies across the site according to data sets from the Department of Energy, Environment and Climate Action from 19-53 (score). The biodiversity mapping of the area is generally higher towards the wetland to the north-west, and with pockets of land adjoining the coast to the east also receiving a high biodiversity score. It is noted that there are no VBA Species Survey Records for Threatened Flora or Fauna within proximity to the Site.

Refer to Figure 10 for details.

Key design considerations:

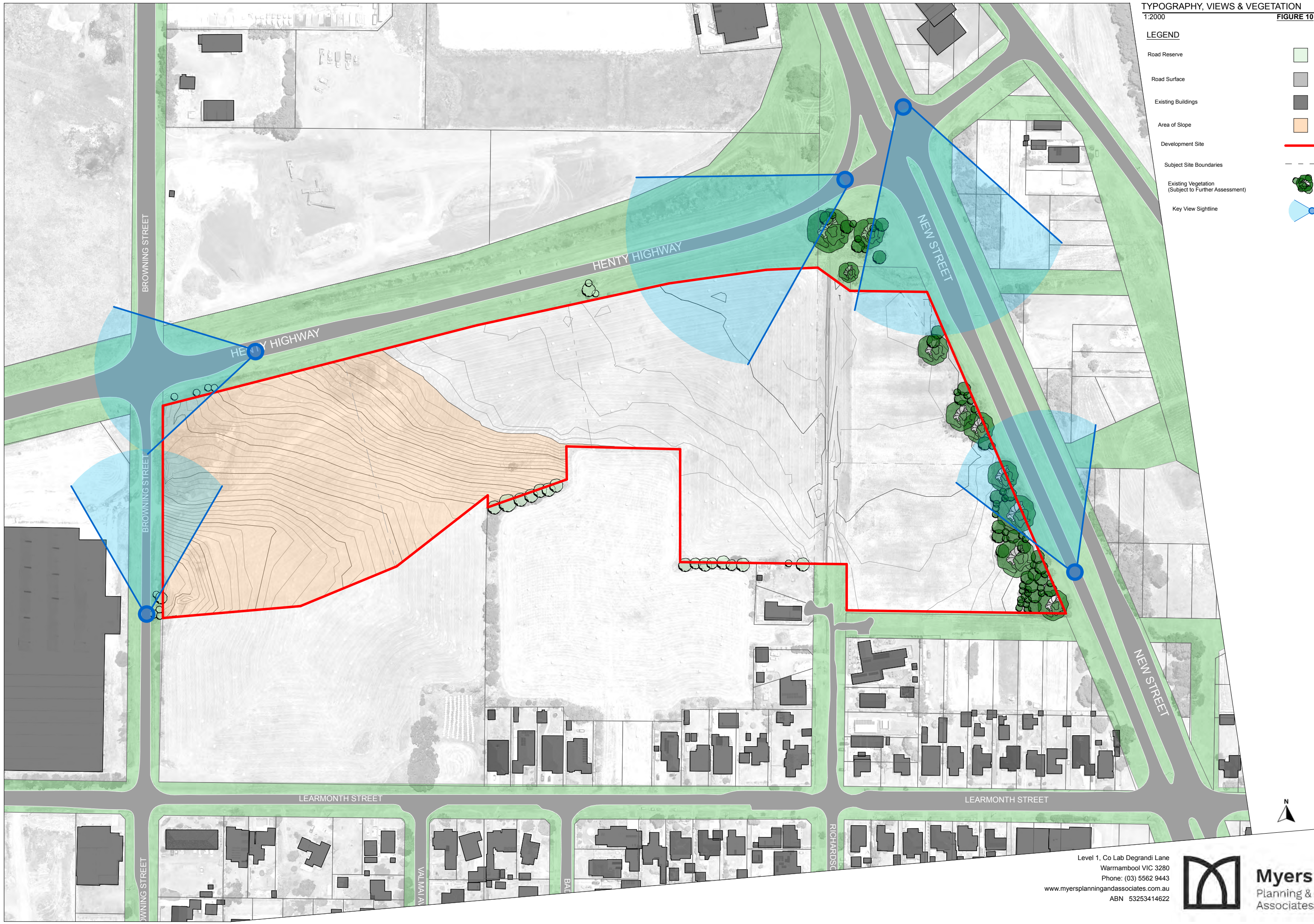
- Ensure that road networks and infrastructure services respond to the fall across the western portion of the site.
- Ensure that any built form within the area of slope appropriately provides for attractive soil retention without excessive site cut (such as incorporating into landscape feature).
- Ensure that stormwater drainage takes the topography of the land into account through its design.
- Consider opportunities for appropriate signage location and wayfinding with visibility from key sightlines.
- Encourage built form, which is softened with vegetation, wherever practicable from key view lines from public land to the precinct.
- Ensure that any buffer or landscape treatments are visually attractive.
- Encourage the retention of existing native trees with a high protection value, where practicable.
- Encourage opportunities for tree planting around the perimeter of the precinct, wherever practicable, to assist with the softening of future development.
- Utilise appropriate aquatic vegetation species if utilised in any constructed drainage or water features.
- Encourage hardstand and parking areas to incorporate water-sensitive urban design measures.

Guidelines for planning permit applications

- Applications for subdivision or development must be accompanied by a Biodiversity Report which assesses any impacts to native flora or fauna on-site or within road reserves as relevant.
- Applications for development should be accompanied by a Landscape Plan which seeks to incorporate landscaping treatments into the design of new development.

LEGEND

- Road Reserve
- Road Surface
- Existing Buildings
- Area of Slope
- Development Site
- Subject Site Boundaries
- Existing Vegetation (Subject to Further Assessment)
- Key View Sightline



5.5. Drainage, Catchment and Waterways

The development plan area slopes from south-west to north-east, with water draining consistently towards the north-east as part of a single catchment and currently utilising an existing swale drain along the northern edge of the site. There is a natural low point and informal basin at the Henty Highway and New Street intersection, marked as Thomas Street, where the storm water then traverses through an outfall drainpipe to Portland Bay after undergoing treatment. Water is also channelised to the north of the subject site through table drains towards the New Street existing Council drainage network.

Existing drainage infrastructure is limited to shallow open swale drains surrounding the subject site. Glenelg Shire Council asset records show drainage infrastructure in the form of pits and underground pipes to the east of the subject site crossing New Street along with balance pipes under Henty Highway from the existing detention basin on the north of Henty Highway.

Council has received a proposal for a 21-lot subdivision at 56 Learmonth Street. This proposal recommends a new reticulated drainage network that not only services the 21 lots proposed, but also addresses drainage solutions for Richardson Street (north). The proposed drainage design traverses the subject site, following the natural topography of land in a northerly direction through a future proposed road reserve to the Henty Highway, then traveling in an easterly direction to the existing drainage infrastructure crossing New Street.

See **Appendix B** – Infrastructure Services Report for further details on the layout of existing and proposed stormwater assets associated with recent developments within and adjoining the Portland Gateway Precinct.

Refer to Figure 11 for details.

Key design considerations:

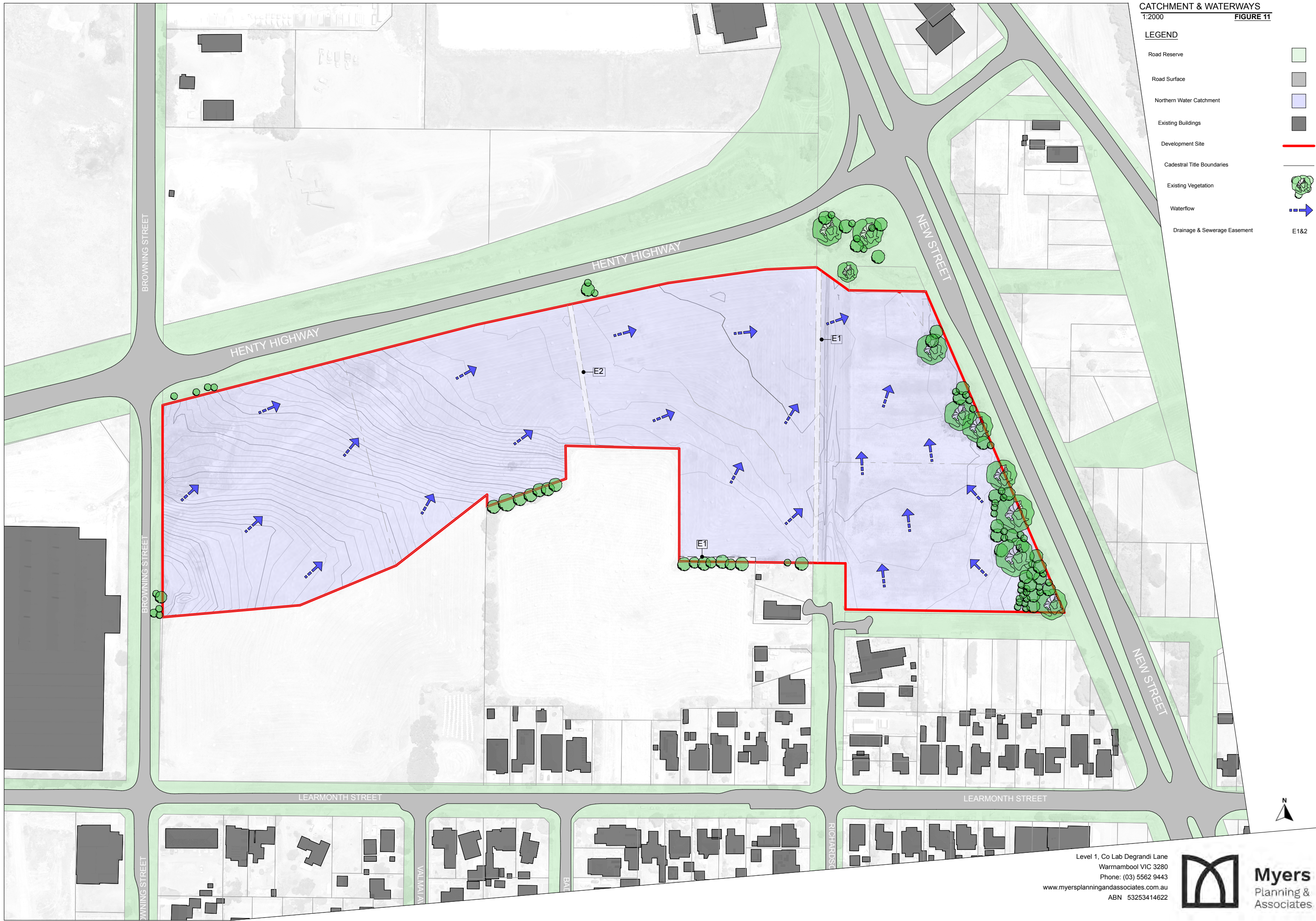
- Ensure that drainage appropriately responds to the topography of the land and reserve lower lying areas for retention, if required.
- Ensure that the road network follows natural drainage lines and topography, where practicable.
- Encourage water sensitive urban design principles throughout the development plan area.
- Ensure that any constructed drainage basins or features are safely built and maintained.
- Explore opportunities to re-use water and encourage rainwater harvesting.

Guidelines for planning permit applications

- Application for subdivision and development should be accompanied with a Stormwater Management Plan which sets out a concept design for all drainage infrastructure to ensure conveyance of the minor and major storm events in accordance with Glenelg Shire Council standards.

LEGEND

- Road Reserve
 - Road Surface
 - Northern Water Catchment
 - Existing Buildings
 - Development Site
 - Cadastral Title Boundaries
 - Existing Vegetation
 - Waterflow
 - Drainage & Sewerage Easement
- E1&2



5.6. Transport and Access

Roads

Henty Highway

The Henty Highway is an arterial highway that runs through the south-western region of Victoria, Australia. It connects the city of Portland, located on the state's south-western coast, to the Western Highway, which runs eastward to Melbourne.

The Henty Highway begins in Portland and heads north, passing through several small towns and agricultural areas before reaching the Western Highway near the city of Horsham. The highway is approximately 200 kilometres long and is a major route for freight traffic as well as tourist traffic.

As it passes through Portland, the Henty Highway turns to bypass the city centre. Through the entry to Portland, it also links with the Princes Highway to the east and the South Australian border to the west. The highway is maintained by State Government and in parts features two lanes of traffic in each direction, with a speed limit of 80 kilometres per hour within the Portland Urban area and 100 kilometres per hour outside of the urban area.

The Henty Highway does not currently have any access points to the development plan area.

New Street

New Street is an Urban Link between Henty Highway and the Portland CBD which runs north-south to the east of the development plan area. New Street currently does not have any access to the development plan area and includes a median strip, preventing any vehicle turning movements across the road reserve.

Browning Street

Browning Street is an Urban Access Street which runs north-south to the west of the development plan area. The development plan area has an existing accessway to Browning Street towards the intersection with Henty Highway. Browning Street contains a crest between Learmonth Street and Henty Highway.

Richardson Street

Richardson Street is a north-south Urban Access Street which extends from Garden Street through to the south of the development plan area. Within proximity to the development plan area, Richardson Street has an intersection with Learmonth Street and provides for access to Cope Street. To the north of Learmonth Street, Richardson provides access to several residential properties. The road in this section is all-weather but is not fully constructed with kerb and channel.

Cope Street

Cope Street is a short stretch of unmade road reserve which sits to the south of the development plan area between Richardson Street and New Street. One property currently obtains access via Cope Street; however, multiple vacant residential lots potentially also require access from Cope Street. Cope Street is only planned to only have access from Richardson Street.

Thomas Street

Thomas Street is demarcated on the certificates of title for the development plan area. It is an unconstructed road reserve in the north-eastern corner of the site at the junction of Henty Highway and New Street. This road reserve is zoned as Transport Zone 2 and contains drainage infrastructure.

Learmonth Street

Learmonth Street is an Urban Access Street which provides east to west access across Portland's north. Learmonth Street comprises residential development between New Street and Browning Street. Land west of Browning Street contains industrial land uses. With Learmonth Street classified as Urban Access, it is not suitable for heavy vehicles accessing the industrial area through the residential section of Learmonth Street, Learmonth Street does not currently provide for any pedestrian footpaths within its road reserve.

Transport

Bus

The Development Plan area is located within 400 metres of the nearest bus stops located on Learmonth Street and Richardson Street as part of the Portland North Bus Route 1 – providing for a loop bus service back to the Portland CBD. At the time of writing the bus service frequency is once each hour.

Trains / Trams

The Development Plan area and Portland more broadly is not serviced by train or tram services for public transport purposes, other than the cable tram for tourism.

Bicycle

There are no dedicated bicycle lanes or infrastructure near the subject Site other than nominated painted lanes within the New Street and Henty Highway road reserves. There is no existing bicycle parking within proximity to the development plan area.

Private vehicle.

Private vehicle movements dominate the mode of travel within Portland and are expected to comprise most trips to the development plan area post development. The road network provides for smooth movement connections for vehicles in a north-south direction (New Street) as well as an east west direction (Henty Highway). Signalised intersections are limited in this area of Portland and the road reserve treatments reflect the dominance of the motor vehicle, including for freight and logistics.

Footpaths

There is a limited footpath network within proximity of the development plan area. Learmonth Street, New Street, Henty Highway and Browning Street all do not provide for a footpath for formal pedestrian access. The nearest footpaths to the south have been shown within the Transport and Access Plan to show how any future footpath network may be able to eventually connect with the existing infrastructure.

See **Appendix B** – Infrastructure Services Report for a detailed analysis of road infrastructure assets.

Refer to Figure 12 for details.

Key design considerations:

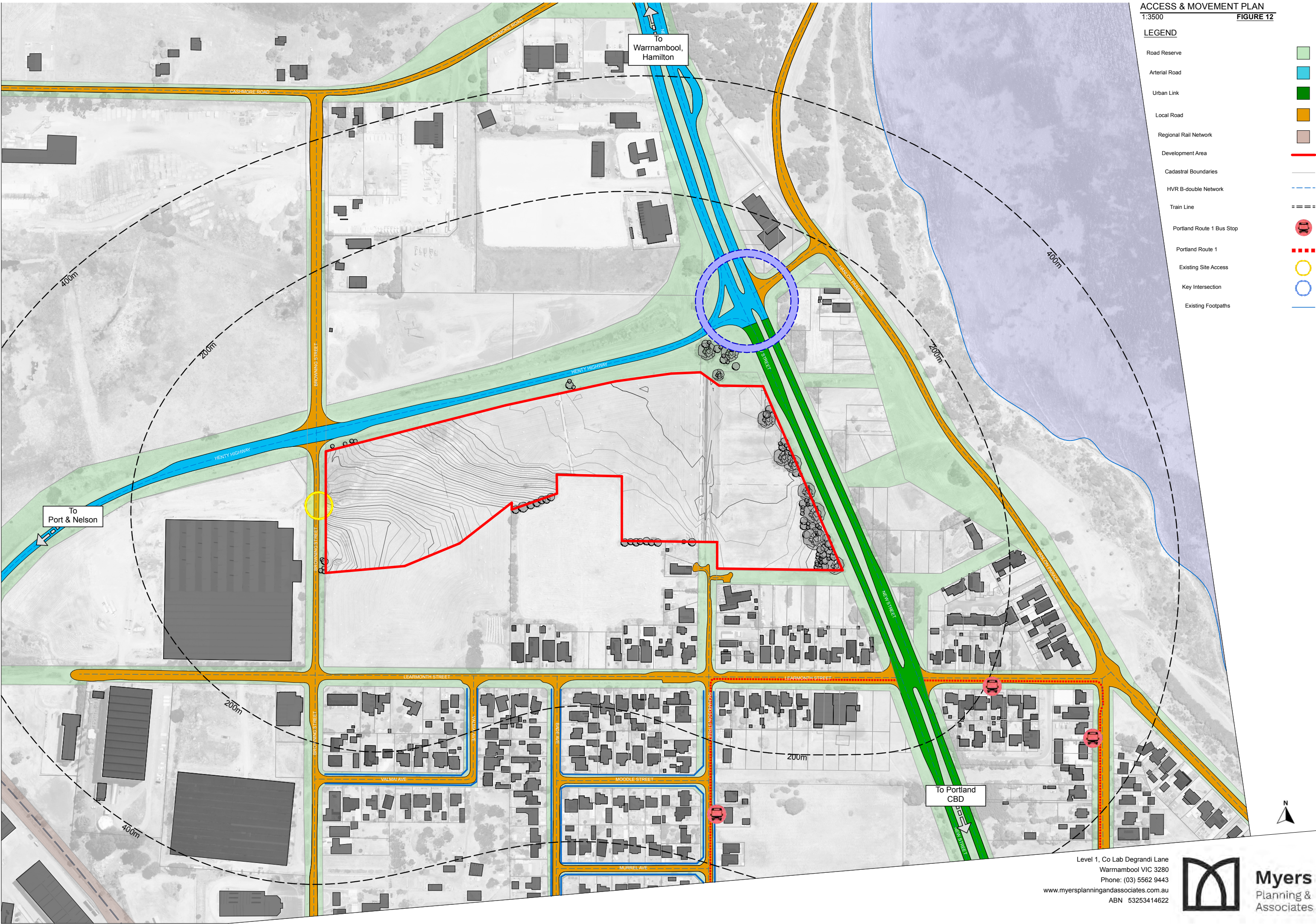
- Explore opportunities for an intersection with the Henty Highway.
- Support a minimal internal road network if it is demonstrated that it can retain functionality and movement for vehicles and B-double trucks.
- Provide for a through-road network from east to west (i.e.: New Street to Browning Street) instead of a loop road only utilising one road frontage.
- Explore opportunities for the retention of opportunities for future footpath linkages to the existing network.
- Encourage bicycle infrastructure and public transport options within the Development Plan area.

Guidelines for planning permit applications

- Applications for subdivision or development should be accompanied with a Traffic Engineering Assessment (or similar), where appropriate, which identifies existing traffic conditions, assesses suitability of proposed internal movement network and access arrangement, and evaluates traffic impact on the surrounding road network.
- Applications for subdivision should be accompanied by a Functional Layout Plan, where appropriate, which provides for cross-sections and details of any proposed road network and how the layout will allow for any infrastructure services proposed.

LEGEND

- Road Reserve
- Arterial Road
- Urban Link
- Local Road
- Regional Rail Network
- Development Area
- Cadastral Boundaries
- HVR B-double Network
- Train Line
- Portland Route 1 Bus Stop
- Portland Route 1
- Existing Site Access
- Key Intersection
- Existing Footpaths



5.7. Bushfire, Heritage and Geology

Bushfire Prone Area

The subject site is entirely mapped within the Bushfire Prone Area which signifies that the site is within an area which is subject to or likely to be subject to bushfires. This area has been mapped in accordance with the appropriate methodology to determine bushfire risk which includes both surrounding vegetation, prevalence of urban areas and topography.

Bushfire risk is the likelihood and consequent of a fire starting, spreading and impacting on people, property and the environment.

The primary risk for the subject site within the Bushfire Prone Area is a grassfire from the north and north-west.

Specific construction and town planning requirements are applicable to these formally designated Bushfire Prone Areas.

The topography of the Site comprises relatively flat areas with the exception of low slope within the south-west of the Site. The land adjoins residential development to the south and east, while there is industrial development to the west and north.

All planning and decision making under the *Planning and Environment Act 1987* relating to land that is within a designated bushfire prone area must consider Clause 13.01-2S 'Bushfire Planning', as appropriate.

Accordingly, an assessment of risk (landscape and site) commensurate with the scale and location is summarised below:

Landscape bushfire risk

- There is little high-threat vegetation beyond 100 metres of the site (except grasslands and modified patches of woodland).
- Extreme bushfire behaviour is not possible due to the modified condition of the surrounding landscape.
- The type and extent of vegetation is unlikely to result in neighbourhood-scale destruction of property.
- Immediate access is available to a place that provides shelter from bushfire (parks, ovals and residential areas).

Bushfire hazard site assessment

- The land is located within a Bushfire Prone Area.
- There is no Bushfire Management Overlay present on the site or surrounding grassland.
- The topography of the land surrounding the subject site is classed as flat/upslope to the north, east and west.
- The subject land is near roads and buildings.

The above notwithstanding, the low fire risk from ember attack and management of any grassland during the fire danger period should be considered as part of the management of subsequent lots to be developed.

Aboriginal Cultural Heritage Sensitivity

The north-eastern corner of the site is identified as being an 'area of cultural heritage sensitivity' pursuant to the *Aboriginal Heritage Regulations 2018*. These areas relate to landforms and soil types where Aboriginal places are more likely to be located.

A Cultural Heritage Management Plan (CHMP) is required for an activity if the activity is a 'high impact activity' and falls in whole or part within an area of cultural heritage sensitivity.

As the site is partially covered by this mapping, this will need to be considered as part of any future land use, subdivision and development.

Geology and soils

The Site is within a Volcanic Plain landform which sits about the coastal complex (which describe the parts of the Shire closer to sea level). Further, soils of the area are mapped to be Chromosols for the locality of the Site within Portland – a subsoil not sodic or strongly acidic.

Coastal Acid Sulphate Soils (CASS) are not expected to be present according to State Government Data sets for this locality of Portland and are unlikely to be found or to impact any future development.

With respect to land contamination no formal testing has been undertaken (such as a Preliminary Risk Screen Assessment); however, this is not expected to be a constraint to the site given that it is zoned for commercial development.

A desktop assessment finds that the site has been vacant and previously was used for agriculture, with a low risk for contamination.

Sensitive uses are not anticipated within the Development Plan Background Report nor under the Commercial 2 Zone itself, without a planning permit.

Should any sensitive uses be proposed in the future it may be appropriate to undertake further testing and analysis with respect to land contamination.

Refer to Figure 13 for details.

Key design considerations:

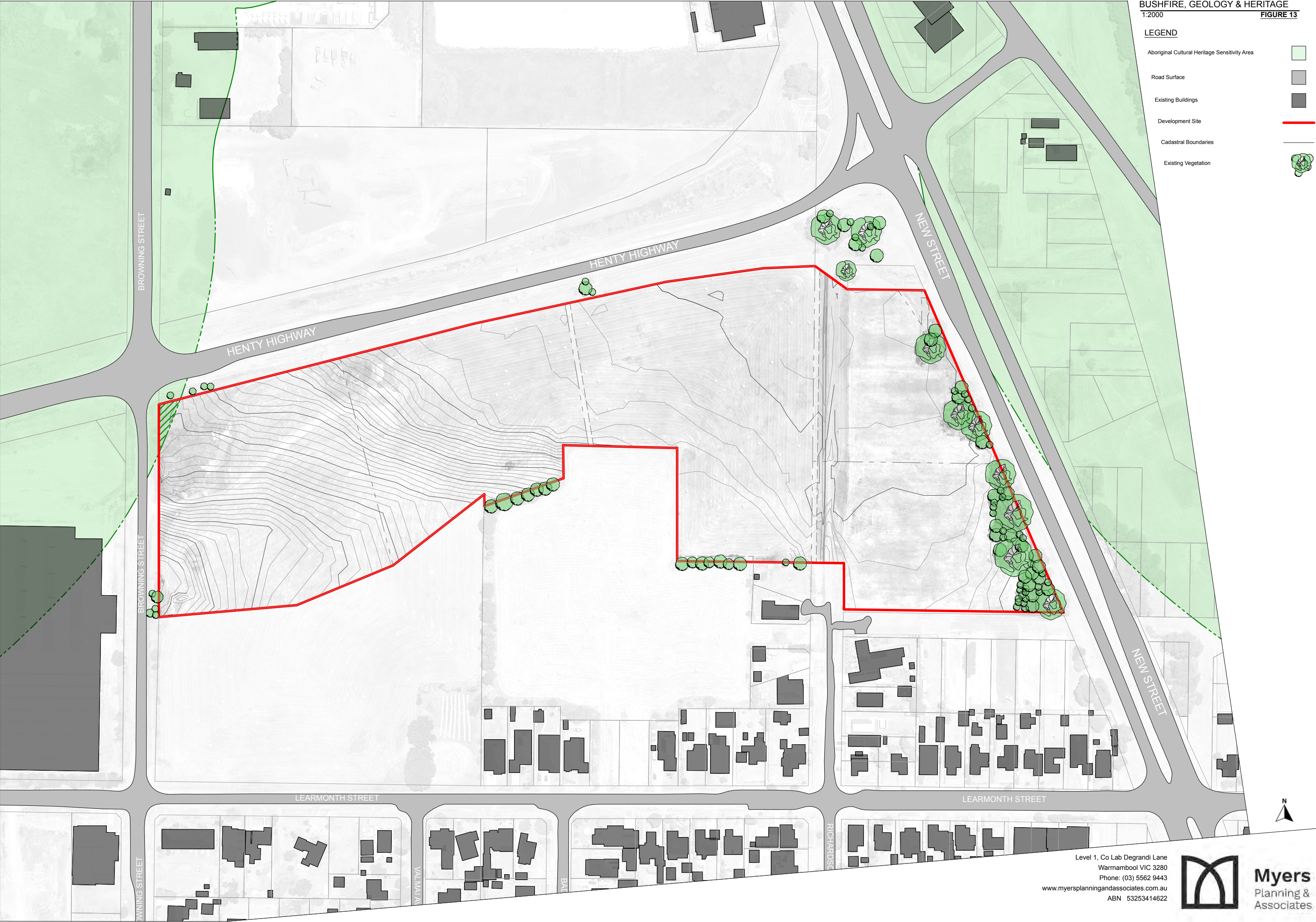
- Retain and protect areas of Aboriginal Cultural Heritage significance.
- Ensure that road networks provide for the requirements of emergency authorities.

Guidelines for planning permit applications

- For planning applications on Lot 1 PS406253H which propose a high impact activity as defined under the *Aboriginal Heritage Regulations 2018*, the provision of either a Cultural Heritage Management Plan, or targeted cultural heritage advice which provides evidence why a Cultural Heritage Management Plan is not required.

LEGEND

- Aboriginal Cultural Heritage Sensitivity Area
- Road Surface
- Existing Buildings
- Development Site
- Cadastral Boundaries
- Existing Vegetation



6. Design Response

The Design Response considers the existing conditions of the locality and the Site Analysis provided within this document and has logically produced the Overall Development Plan (Figure 15).

The following section has regard to the requirements of Schedule 10 to the Development Plan Overlay (DPO10) which includes a summary on how the development plan responds to:

- Roads, infrastructure and staging.
- Land use and built form.
- Landscaping and interface

6.1. Roads, Infrastructure and Staging

Roads

The proposed movement network is consistent with the objectives of DPO10 as it facilitates the efficient movement of vehicles and pedestrians. The development plan incorporates a key east-west linkage to allow for appropriate vehicle movements from New Street through to Browning Street.

The location of the New Street entrance intersection creates an opportunity for an attractive gateway to the future commercial development site.

If proposed to be a government road, it is to be consistent with the Infrastructure Design Manual, providing a 25-metre reserve width (Industrial Street) which will allow for appropriate vehicle movements (including medium and heavy rigid vehicles) in addition to parallel car parking spaces within the carriageway. A cross section of this road typology is provided at Figure 1.

All roads within the site are to be sufficient to adequately facilitate relevant service and emergency vehicles.

The intersection at New Street will require modification to the turning lanes and median strip to provide for satisfactory ingress and egress to the Site.

The Browning Street intersection has been located at the southern aspect of the Site due to the topography of the locality – where there is a crest between Henty Highway and Learmonth Street. The highpoint is located approximately where the intersection is proposed.

The development plan nominates a potential intersection with Henty Highway to create additional highway connections and to improve truck movements into the Site. This intersection will be to the satisfaction of the Department of Transport and Planning.

The development plan makes a reference to the key intersection of Henty Highway and New Street. The Green Triangle Freight Action Plan nominates that there are to be intersection improvements at this location. The upgrade of this intersection would be of benefit inasmuch that it would likely increase the safety and functionality of the intersection for vehicles.

Pedestrian movement

The proposed road network provides for a footpath on either side for safe pedestrian movement internally of the Site. Future development will be able to link to these paths to enable safe pedestrian access to any front entries.

Whilst there is no proposed linkage through to Richardson Street to the south, future opportunities exist for connections to the Site along Browning Street and New Street which would link to the proposed road network.

Infrastructure

The stormwater storage systems for future development on the Site will be designed so that water discharging from the site will be restricted to pre-development AEP levels as designated by the local authority, Glenelg Shire Council. The development plan identifies a storage basin or gross pollutant trap location at the low point of the Site within the north-eastern corner.

Detailed design of stormwater will be left to future planning applications for subdivision or development.

Further, the proposed road layout has been proposed to be co-located with the likely locations of future infrastructure. The road layout working with the topography of the land where possible and being located within proximity to existing services. The Site is capable of being connected to the required utilities to service subdivision and development (such water, sewer, electricity and if required gas).

See Appendix B (Infrastructure Services Report) for further details.

Wannon Water have indicated that the development plan area will be serviced for water supply from the DN225-millimetre mains to the west and will be serviced for sewer by the mains to the north.

Future planning applications will be required to be accompanied by an engineering services report or functional layout plan to demonstrate how the development or subdivision can respond to the provision of services.

Staging

Staging is anticipated to progress from east to west, from New Street to Browning Street with the associated delivery of infrastructure such as intersections, internal roads and drainage to be accounted for at the time of subdivision or development.

The earlier stages will require the consideration of the initial intersection to New Street and provide for stormwater system improvements, likely in an interim format until such time additional development warrants further upgrade.

The later stages are expected to complete road connections for the development to the western end at Browning Street.

Interim and ultimate road and stormwater solutions will be staged in agreement with Glenelg Shire Council, with the implementation to be commensurate to the vehicle traffic generation and stormwater run-off required for each subdivision or development.

6.2. Land Use and Built Form

Land Use

Land use and development areas have been nominated within the Site to generally indicate how the land is planned to be developed in the future. The development plan does not further dictate the types of land uses other than to encourage land uses which facilitate large-format commercial premises in line with the vision and objectives at Section 1.3 of this document.

Built Form and design

Future development applications will be guided by the Commercial 2 Zone in addition to other requirements of the Glenelg Planning Scheme. The development plan nominates the location where development can be sited. To create an identifiable character and a high quality attractive the built form, the built form should consider the design guidelines within this development plan and at Section 7.0. These further design considerations for built form will encourage good urban design outcomes and an appropriate character.

Signage

Being a large commercial development site with good road user visibility, it is highly likely that signage will be proposed to identify businesses and to attract customers.

The development plan proposes two (2) potential locations for signage. The signage locations are at key intersections with either Henty Highway or New Street. These signage locations take advantage of the entrance location to Portland.

This signage is anticipated to be in the form of pole or sky signs, suitable for illumination, which identify the collective businesses within the Site.

Signage requirements are at Clause 52.05 of the Glenelg Planning Scheme. The subject site within the Commercial 2 Zone is in Category 1 for signage.

Signs and light spill should not be directed towards the residential areas. Signs are to be non-reflective, not contain flashing lights, and not mimic or be mistaken for traffic control.

6.3. Landscaping and interface

Landscaping

The development plan area has several interfaces which require an appropriate design response. Landscaping should be selected to provide for softening as well as to improve the overall aesthetic of the commercial development site. Landscaping should also be incorporated within car parking areas as part of future development.

Landscaping should retain existing species (where appropriate) and incorporate additional indigenous species from Victorian Volcanic Plain EVC23 where appropriate.

Street trees are also to be planted within the road reserve, where appropriate in accordance with the typical cross-section of the 25-metre industrial street at Figure 14.

Sensitive use interface

The development plan notes the sensitive interface between the development plan area and the residential area to the south. Commercial development is to provide for noise attenuation in the form of a wall or landscape buffer, where appropriate. The details of appropriate treatments can be determined by a future acoustic report prepared by a qualified person.

Planting of indigenous species can be incorporated to improve the aesthetic of the noise attenuation measures.

The presence of adjacent existing and future road reserves also provides for adequate separation from residential properties, particularly on parts of the southern and western sections of the development plan area.

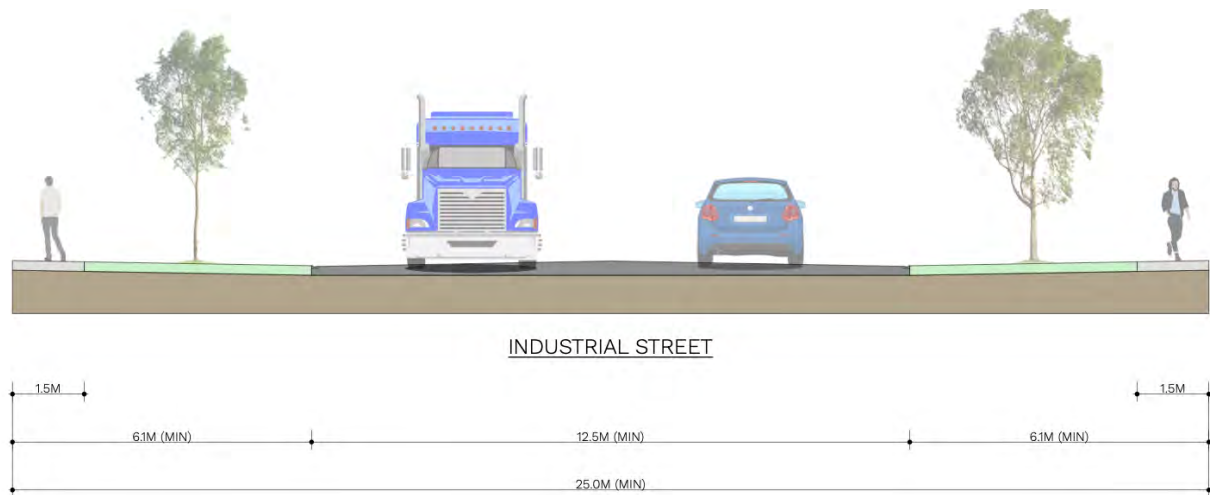


Figure 14: Typical Industrial Street Cross section

LEGEND

-



7. Design Guidelines

In addition to Glenelg Planning Scheme requirements, any future planning application should consider the following design guidelines where appropriate, which have been drafted to meet the objectives and intentions of Schedule 10 to the Development Plan Overlay.

Built Form

Scale and Massing: Consider the scale and massing of the building to ensure that it is proportionate and fits within its context. Variations in building heights, setbacks and articulation should be considered, wherever practicable. If excavation is required for development on account of site topography, then these areas should also be made attractive using retaining walls and landscaping so that the development sits well within the landscape.

Visual Interest: Development should incorporate visual interest into the building's design as much as practicable. This can include the use of mixed construction materials, colours, textures, and patterns, as well as landscaping.

Active frontages

Street-Level Transparency: Consider Incorporating large windows and glass facades to allow for visibility into the building and promote interaction between the occupants and the street. This can help to create a sense of openness and accessibility, as well as promote safety and security.

Pedestrian Access: Developments should consider providing multiple points of pedestrian access to the site or building from the street network. Consider designing the building to be set back from the street to create a welcoming and accessible entryway.

Retail and Commercial Spaces: Include retail and commercial spaces at ground floor to activate the street and create a vibrant pedestrian environment. Consider designing these spaces to have large store fronts and be accessible from the street and associated car parking areas.

Public Amenities: Incorporate public art and amenities where possible, such as seating, planters, and lighting, to enhance the pedestrian experience and create a sense of place.

Safety and Security: Design buildings to promote safety and security, such as using lighting, wayfinding signage surveillance cameras, and landscaping to enhance user experience.

Environmentally Sustainable Design

Energy Efficiency: Incorporate energy-efficient measures into the design, such as LED lighting, energy-efficient HVAC systems, and energy-efficient building materials. Consider using renewable energy sources, such as solar panels, to power the building.

Sustainable Materials: Buildings should use sustainable building materials, such as recycled content, FSC-certified wood, and low-VOC paints and finishes. Consider using materials that have been locally sourced to reduce transportation emissions.

Waste Reduction: Waste reduction measures should be incorporated into the design, such as recycling and composting programs, and consider designing the building with the capacity to recycle and compost materials.

Water Sensitive Urban Design

Stormwater Management: Proposals should Incorporate stormwater management strategies, such as rain gardens, and permeable pavements, to capture and treat stormwater on-site. This can help to reduce the volume and velocity of stormwater runoff, prevent flooding and erosion, and improve water quality.

Water Sensitive Landscaping: Proposals should Use water-sensitive landscaping, such as native plants and trees, to reduce the need for irrigation and promote biodiversity. Consider designing the landscape to capture and store rainwater, such as through bioswales or rain gardens.

Water Recycling: Consider incorporating water recycling systems, such as greywater or blackwater reuse systems, to reduce the demand for potable water and minimise wastewater discharge. These systems can capture and treat water from sinks, showers, and washing machines for reuse in non-potable applications.

Water-efficient Fixtures: New development should install water-efficient fixtures, such as low-flow or dual flush toilets, faucets, and showerheads, to reduce water consumption and minimise wastewater generation.

Appendix A

REGISTER SEARCH STATEMENT (Title Search) Transfer of Land Act 1958

Page 1 of 3

VOLUME 10476 FOLIO 172

Security no : 124103773564C
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LAND DESCRIPTION

Lot 1 on Plan of Subdivision 406254F.

PARENT TITLES :

Volume 10386 Folio 395

Volume 10394 Folio 315 to Volume 10394 Folio 319

Volume 10394 Folio 798 Volume 10415 Folio 381 Volume 10471 Folio 656

Volume 10471 Folio 683 to Volume 10471 Folio 685

Volume 10471 Folio 687 to Volume 10471 Folio 693

Created by instrument PS406254F 27/10/1999

REGISTERED PROPRIETOR

Estate Fee Simple

Sole Proprietor

LORD BUDDHA PTY LTD of LEVEL 1, 501 CHURCH STREET RICHMOND VIC 3121

AE969659C 24/03/2007

ENCUMBRANCES, CAVEATS AND NOTICES

STATUTORY CHARGE Section 96(1) Land Tax Act 2005

AJ499104B 20/02/2012

MORTGAGE AE969661R 24/03/2007

BENDIGO BANK LTD

COVENANT as to part 1349140

COVENANT as to part 1742782

COVENANT as to part 2304549

CAVEAT AJ024129M 23/06/2011

Caveator

ICR PROPERTY INVESTMENT GROUP PTY LTD

Grounds of Claim

CHARGE CONTAINED IN AN AGREEMENT WITH THE FOLLOWING PARTIES AND DATE.

Parties

THE REGISTERED PROPRIETOR(S)

Date

21/06/2011

Estate or Interest

INTEREST AS CHARGE

Prohibition

ABSOLUTELY

Lodged by

POINTON PARTNERS

Notices to

POINTON PARTNERS of LEVEL 2 640 BOURKE STREET MELBOURNE VIC 3000

CAVEAT AJ024198R 23/06/2011

Caveator

DALL HOLDINGS PTY LTD

Grounds of Claim

CHARGE CONTAINED IN AN AGREEMENT WITH THE FOLLOWING PARTIES AND DATE.

Parties

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DAVID RODDEN
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Caveator
BUNNINGS PROPERTIES PTY LTD ACN: 008557622
Grounds of Claim
PURCHASERS' CONTRACT WITH THE FOLLOWING PARTIES AND DATE.
Parties
THE REGISTERED PROPRIETOR(S)
Date
04/02/2022
Estate or Interest
FREEHOLD ESTATE
Prohibition
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Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan set out under DIAGRAM LOCATION below.

DIAGRAM LOCATION

SEE PS406254F FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: HENTY HIGHWAY PORTLAND VIC 3305

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ADMINISTRATIVE NOTICES

NIL

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COVENANT as to part C218457

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ACTIVITY IN THE LAST 125 DAYS

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Street Address: 68 RICHARDSON STREET PORTLAND VIC 3305

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LAND DESCRIPTION

Lot 1 on Plan of Subdivision 406253H.

PARENT TITLES :

Volume 10386 Folio 384 to Volume 10386 Folio 386

Volume 10459 Folio 718

Created by instrument PS406253H 23/08/1999

REGISTERED PROPRIETOR

Estate Fee Simple

Sole Proprietor

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AE969659C 24/03/2007

ENCUMBRANCES, CAVEATS AND NOTICES

STATUTORY CHARGE Section 96(1) Land Tax Act 2005
AJ499101H 20/02/2012

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DIAGRAM LOCATION

SEE PS406253H FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NIL

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Additional information: (not part of the Register Search Statement)

Street Address: HENTY HIGHWAY PORTLAND VIC 3305

ADMINISTRATIVE NOTICES

NIL

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LAND DESCRIPTION

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PARENT TITLES :

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Volume 10459 Folio 718

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AE969659C 24/03/2007

ENCUMBRANCES, CAVEATS AND NOTICES

STATUTORY CHARGE Section 96(1) Land Tax Act 2005
AJ499102F 20/02/2012

MORTGAGE AE969661R 24/03/2007
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Grounds of Claim
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DIAGRAM LOCATION

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ACTIVITY IN THE LAST 125 DAYS

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: HENTY HIGHWAY PORTLAND VIC 3305

ADMINISTRATIVE NOTICES

NIL

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LAND DESCRIPTION

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Volume 10459 Folio 718

Created by instrument PS406253H 23/08/1999

REGISTERED PROPRIETOR

Estate Fee Simple

Sole Proprietor

LORD BUDDHA PTY LTD of LEVEL 1, 501 CHURCH STREET RICHMOND VIC 3121
AE969659C 24/03/2007

ENCUMBRANCES, CAVEATS AND NOTICES

STATUTORY CHARGE Section 96(1) Land Tax Act 2005
AJ499103D 20/02/2012

MORTGAGE AE969661R 24/03/2007
BENDIGO BANK LTD

CAVEAT AJ024129M 23/06/2011
Caveator
ICR PROPERTY INVESTMENT GROUP PTY LTD
Grounds of Claim
CHARGE CONTAINED IN AN AGREEMENT WITH THE FOLLOWING PARTIES AND DATE.
Parties
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Date
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Estate or Interest
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Caveator
DALL HOLDINGS PTY LTD
Grounds of Claim
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Parties
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CAVEAT AJ024218N 23/06/2011

Caveator

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Parties

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Date

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Estate or Interest

INTEREST AS CHARGE

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DIAGRAM LOCATION

SEE PS406253H FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: HENTY HIGHWAY PORTLAND VIC 3305

ADMINISTRATIVE NOTICES

NIL

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DOCUMENT END

Appendix B



Myers
Planning & Associates

Infrastructure Services Report

Portland Gateway Precinct Development Plan

Corner of Henty Highway & New Street, Portland

Prepared for Glenelg Shire Council

Issued
May 2023
www.myersplanning.com.au

Dispensary Lane (Rear of 190 Timor Street)
Warrnambool VIC 3280
(03) 5562 9443

Quality assurance

Infrastructure Services Report

8449 Henty Highway, Henty
Prepared for Brad Urquhart

Project Number

22-1048

Revision

01

Prepared By

JH

Reviewed By

DP

Project Lead

DP

Issued

2 May 2023

Revision	Date	Issue
00	10 March 2023	DRAFT Infrastructure Services Report
01	2 May 2023	Infrastructure Services Report

Acknowledgement of Country



We acknowledge the Traditional Owners of the Country on which this application applies and recognise their continuing connection to the land, water and culture. We pay our respects to their Elders past, present and emerging.

We care about our community



We donate a fixed percentage of our income each year to local organisations that work towards a better world. We also advise and represent a select group of clients on a reduced cost or pro bono basis.



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1 Introduction

1.1 General

This report has been prepared by Myers Planning & Associates on behalf of Glenelg Shire Council for the Portland Gateway Precinct, located south of the Henty Highway, between New Street and Browning Street. (the 'Site').

The report should be read in conjunction with the Portland Gateway Precinct Development Plan Background Report.

This report is informed by:

- a review and investigation of existing conditions and utility service assets in the general vicinity of the Site;
- consultation with Council and relevant authorities regarding servicing strategies to cater for future development of the Site.
- Proposed concept engineering plans by Council and others.
- Proposed concept 'development plans' by others.

1.2 Limitations

This report has considered the following documents:

- Service authority advice (as of February 2023)
- Certificates of Title (dated 8 February 2023)
- Infrastructure Design Manual (dated 24 March 2020)
- Register of Public Roads- Glenelg Shire Council (18 October 2022).
- VicRoads Register of Public Roads (31 January 2015).

2 Site and proposal description

2.1 Site description and title particulars

Site address	Henty Highway and 68 Richardson Street
Title detail	Lot 1, Lot 2 and Lot 3 on Plan of Subdivision 406253H Lot 1 and Lot 2 on Plan of Subdivision 406254F
Site description	<p>The Site is located at Portland's northern gateway into the city, approximately 1.8 kilometres to the north of the Portland Central Business District. Further the Site is located 250 metres west of Portland Bay.</p> <p>The Site is irregular in shape with an area of 7.9 hectares and a frontage of approximately 217 metres to New Street, 520 metres to Henty Highway and 132 metres to Browning Street.</p> <p>The Site is largely vacant of vegetation and comprises undulating land with slope from the south-western corner through to the north-eastern corner.</p>
Landowners	Lord Buddha Pty Ltd
Easements, restrictions or covenants	<p>The site is not encumbered by any restrictions or covenants of the Planning and Environment Act 1987 and the Subdivision Act 1988; however, there are a number of easements on the plan of subdivision as follows:</p> <p><u>PS406254F</u></p> <p>E1: Telecommunications and drainage easement at 3.5 metres wide, along the western boundary, running north-south.</p> <p><u>PS406254H</u></p> <p>E1: Drainage and sewerage easement 3 meters in width for a length of 47.4 meters along the southern boundary of Lot 3.</p> <p>E2: Drainage and sewerage easement 2 metres in width along the western title boundary of Lot 3.</p>



Figure 1: Aerial Image

2.2 Proposal description

The Portland Gateway Precinct Development Plan is to provide guidance for future development applications for use, development and subdivision on the site. The Site is expected to accommodate a range of commercial uses appropriate to the highway context and prevailing planning policy framework (i.e. zones, overlays, and local and state planning policies) (refer to Figure 2).

The focus of Section 3 of this Report is to describe existing development services, assets, and provide guidance on potential options for future servicing of the site.

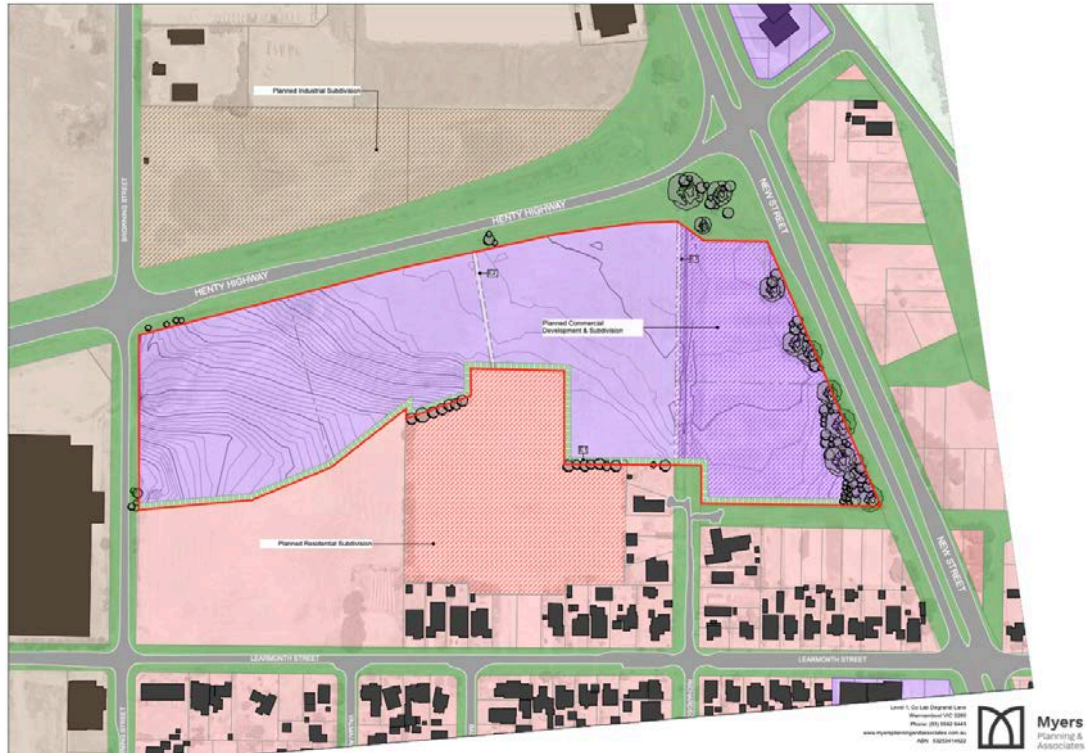


Figure 2: Land Use & Built Form layout (MPG 22-1048)

3 Infrastructure Services

3.1 Road Network

The subject site is located in Portland's northern gateway into the city and has road frontage to the Henty Highway, Browning Street and New Street.

Henty Highway is an Arterial Road (Transport Zone Category 2 (TRZ2), managed by Department of Transport) with an 80kph speed limit along the entire frontage of the site.

Henty Highway has a cross section that comprises fully sealed carriageway of approximately 9.0m width, a formed unsealed shoulder on each side and grassed roadside drainage areas within a road reserve which ranges from 46m to 100m in width. The intersection of Henty Highway and Browning Street has additional pavement width with intersection widening for the intersection including left and right turning treatments (lanes), as seen in Figures 3 & 4 below.



Figure 3: Henty Highway aerial with subject site on the left



Figure 4: Henty Highway with subject site on the left

Browning Street is an Urban collector road, managed by Glenelg Shire Council with a 50kph speed limit along the entire frontage of the site.

Browning Street has a cross section that comprises fully sealed carriageway of approximately 6.0m width, a formed unsealed shoulder on each side and grassed roadside areas within a road reserve approximately 20.1m in width. The topography of Browning Street has a crest centrally located between Learmonth Street and Henty Highway, as seen in Figures 5 & 6 below.



Figure 5: Browning Street aerial with subject site on the left



Figure 6: Browning Street with subject site on the right

New Street is an Urban Link road, managed by Glenelg Shire Council with a 80kph speed limit along the entire frontage of the site.

New Street has a cross section that comprises fully sealed dual carriageway of approximately 7.5m width for north bound traffic. The formation includes kerb and channel and both left turn auxiliary lane and right turn lane, approaching the Henty Highway intersection, as seen in Figures 7 & 8 below.



Figure 7: New Street aerial with subject site on the left



Figure 8: New Street with subject site on the left

3.2 Traffic Volumes

Traffic volume data has been collated from the Glenelg Shire Council for Browning Street, New Street and Henty Highway. The key data findings are summarised in the table, below:

Location	Date	Daily Traffic Volumes			Peak Hour Traffic Volumes		
		Average Weekday	Friday	Saturday	Average Weekday	Friday (PM)	Saturday (Midday)
New Street Inbound, south of Ring Road	Feb/Mar 2022	3,780	4,010	3,530	390	390	360
New Street Outbound south of Ring Road	Feb/Mar 2022	3,700	3,850	3,370	330	320	300
Henty Highway Inbound north of Ring Road	Feb/Mar 2022	5,430	5,620	4,510	460	460	430
New Street Inbound north of Learmonth Street	Apr 2019	3,840	4,040	2,950	N/A	N/A	N/A
New Street Outbound north of Learmonth Street	Apr 2019	3,840	4,180	2,950	N/A	N/A	N/A
Ring Road 200m east of Browning Street	Jan 2019	3,570	3,750	2,290	270	280	180

Figure 9: Traffic Count data, Glenelg Shire Council

3.3 Crash History

A review of the Department of Transport's CrashStats for the last available five years of data indicates that there has been no reported casualty crashes on New Street or Browning Street. However, three crashes have been reported in the last 5 years at the intersection of Henty Highway and New Street as follows:

- a 'right far' type crash on Thursday 18 June 2020 during the daytime resulting in an 'other injury';
- a 'cross traffic' type crash on Sunday 7 October 2019 during the daytime resulting in an 'other injury'; and
- a 'cross traffic' type crash on Friday 19 October 2018 during the daytime resulting in three 'other injuries'.

A 'cross traffic' crash was also reported at the intersection of New Street and Learmonth Street (south of the site) resulting in an 'other injury'. The above-reported crashes do not indicate any obvious crash patterns other than the risk of 'cross traffic' type crashes at unsignalised cross-intersections. The safety risk associated with unsignalised cross-intersections is well known.

3.4 Transport Infrastructure

The Development Plan will need to ensure future subdivision of the site is well-planned and makes provision for well-connected roads, intersections, pedestrian access, drainage and other infrastructure. The site will require a number of new intersections, allowing functional vehicle access to new roads traversing the site. Council's engineers, have identified potential intersection treatment options and suitable locations onto the Henty Highway, New Street and Browning Street as seen in Figure 10 below.

Council road concepts have been reviewed, and while Council's designs are generally in accordance with accepted AustRoad standards and practices, the decision on the ultimate access point will need to be informed by a traffic impact assessment and review by a qualified traffic engineer, at the time of development.

As the Henty Highway is a Department of Transport managed road, early referral advice should be sought to determine the likely range of intersection types acceptable to the road authority. Options to be considered as part of an ultimate access solution should include typical two-way intersection highway treatments as well as one-way service lane treatments at the Henty Highway frontage (northern boundary).

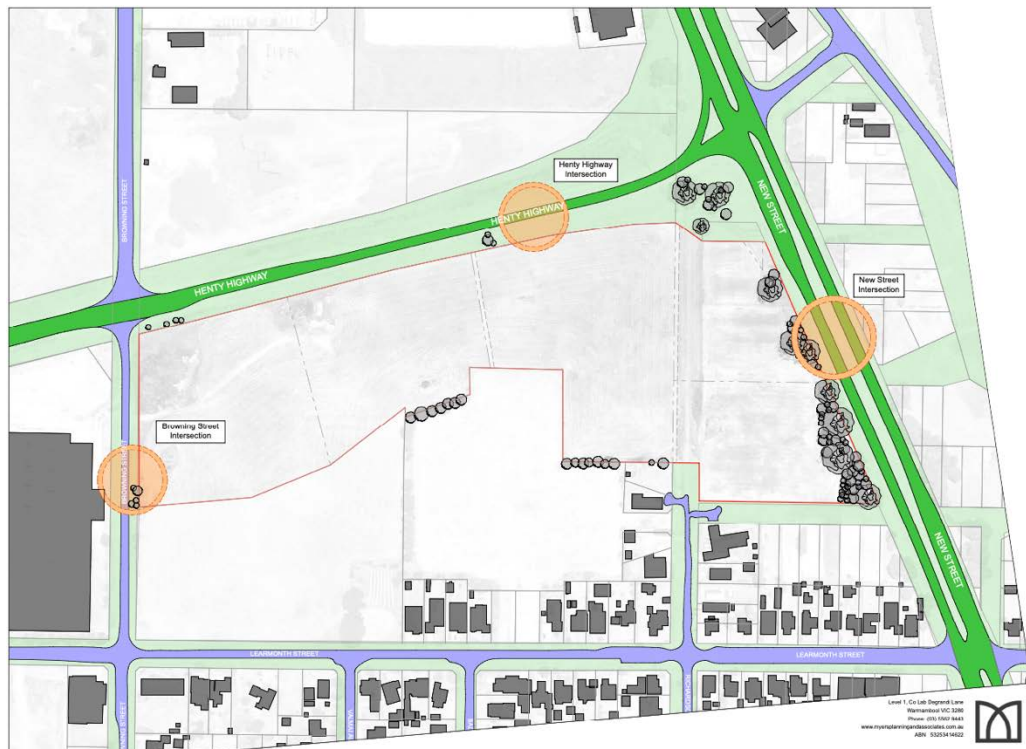


Figure 10: Potential Intersection Locations

The Development Plan should encourage safe and efficient movement for all road users. A well-planned network of internal roads should ensure traffic functionality for any future development.

Should further subdivision be considered, the site should be developed with a legible network of internal roads designed to Council's satisfaction, and as recommended within the Infrastructure Design Manual (see Figure 11).

The Glenelg Planning Scheme, Infrastructure Design Manual (IDM) and various other transport planning texts such as Austroads publications will continue to be referred to for best practice design guidance.

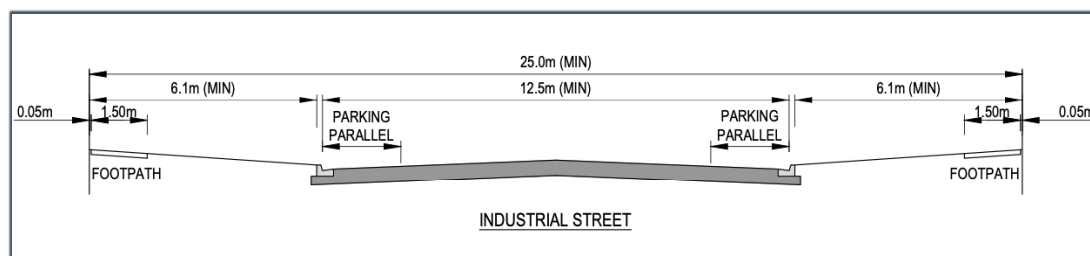


Figure 11: Standard Commercial/Industrial Cross Section (IDM)

3.5 Drainage Network

The site topography has a fall from the southwest to northeast across the site. Stormwater from the site currently flows across the natural path towards the road reserves in Browning Street, Henty Highway and New Streets, where stormwater is channelised through open table drains discharging to the existing Council drainage network under New Street and Lowe Street towards the Southern Ocean.

Existing drainage infrastructure is limited to shallow open swale drains surrounding the subject site. Glenelg Shire Council asset records show drainage infrastructure in the form of pits and underground pipes to the east of the subject site crossing New Street along with balance pipes under Henty Highway from the existing detention basin on the north of Henty Highway as seen in Figure 12 below.

Council has issued a permit for a 21 lot subdivision at 56 Learmonth Street. This proposal recommends a new reticulated drainage network that not only services the 21 lots proposed, but also addresses drainage solutions for Richardson Street (north). This drainage design traverses the subject site, following the natural topography of land in a northerly direction through a future proposed road reserve to the Henty Highway, then traveling in a easterly direction to the existing drainage infrastructure crossing New Street as seen in Figure 12 below. This proposal requires further engineering solutions to ensure its functionality.



Figure 12: Existing & Proposed underground drainage network

The stormwater management plan will compute modelling of catchment area and detail how much stormwater can leave the site and at what flow rate. It will also provide computations on the amount of stormwater that needs to be detained on site. The stormwater runoff will require treatment to achieve best practice Water Sensitive Urban Design guidelines.

The stormwater storage systems for future lots on the site will be designed so that water discharging from the site will be restricted to pre- development annual exceedance probability (AEP) levels as designated by the local authority, Glenelg Shire Council. The preliminary stormwater assessment identifies a storage basin or GPT location at the low point of the subject site as seen in Figure 13 below.

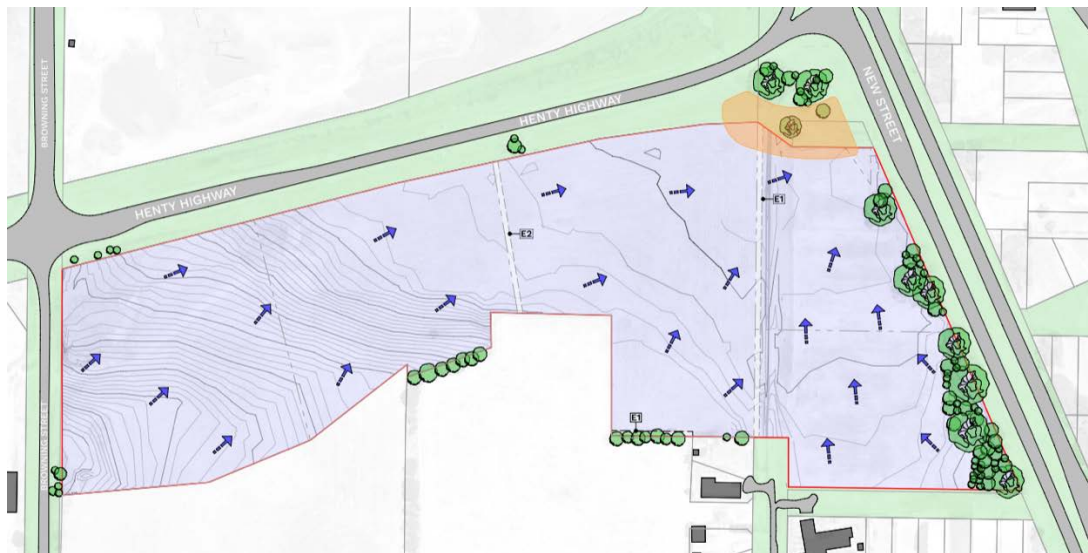


Figure 13: Contour plan & Low point

3.6 Sewerage Reticulation

Wannon Water is the responsible authority for the provision of sewerage reticulation facilities for any development works in Portland. Our enquiries have located an existing sewer rising main on the northern side of Henty Highway, as seen in Figure 14 below. Other existing sewer infrastructure has been identified within Learmonth Street to the south of the site, this 150mm gravity sewer line services existing dwellings fronting Learmonth Street.

A 150mm gravity sewer extension line is proposed for a number of lots within the 21 lot subdivision at 56 Learmonth Street. The proposed sewer design as seen in figure 14 for this 21 lot subdivision delineates the balance of the lots require construction of a new 150mm sewer line traversing the subject site, boring under Henty Highway and connecting into the existing sewer main north of Henty Highway.

Wannon Water have confirmed any further development of lots within the subject site should be serviceable from above main sewer trunk line traversing the site. The existing gravity sewer north of the Henty Highway is at significant depth so a gravity system connecting to the existing system can be designed to service the whole of the area of interest.

It is expected that Wannon Water will advise on sewer servicing requirements as part of agency referrals through the planning permit process and as on any permit granted.

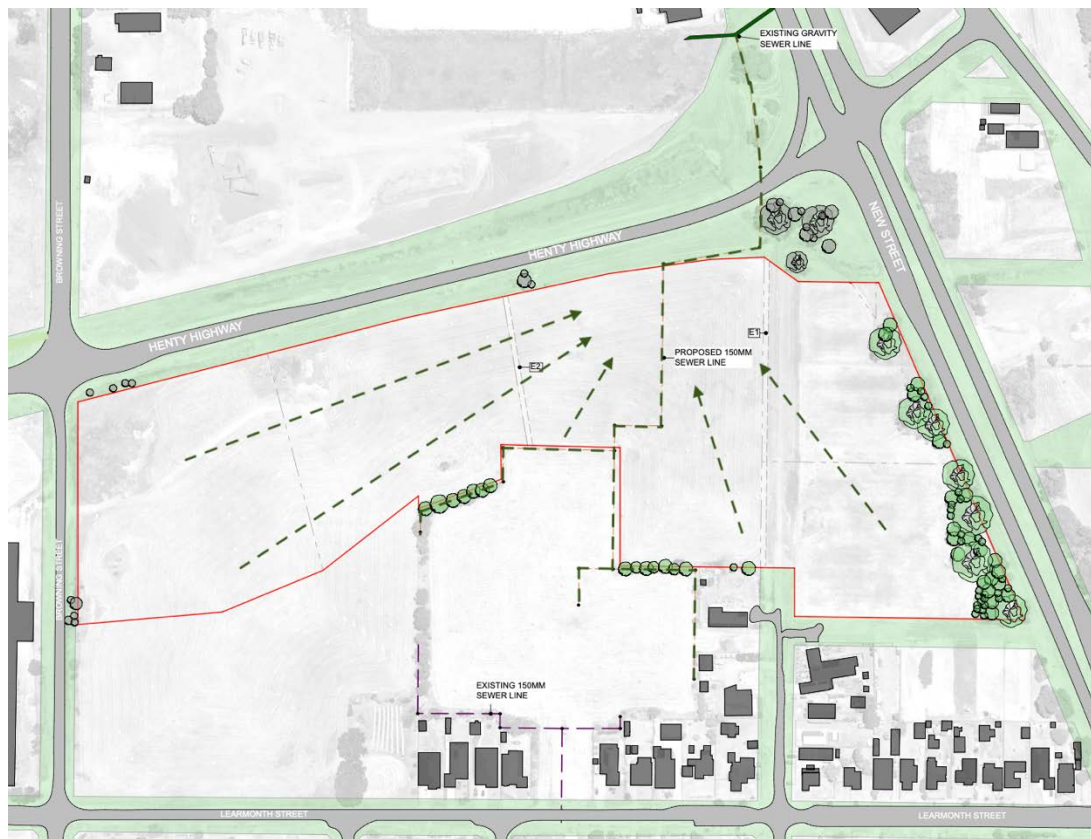


Figure 14: Sewer infrastructure, existing and proposed.

3.7 Water Supply

Wannon Water is the responsible authority for the provision of water supply reticulation facilities for the development. Existing water mains are located on the west of the site in Browning Street with a 225mm main and on both Learmonth Street and Ricardson Streets with a 100mm water line servicing neighbouring properties and hydrant points as shown in Figure 15.

Wannon water has a preference to utilise the DN225mm mains for water supply.

It is expected that a new main branch line will required from existing water main infrastructure traverse the site allowing for individual branches to service each of the Lots with their own metered connection.

Fire hydrant points will also be located along the new branch infrastructure, providing sufficient fire fighting coverage for all proposed lots.

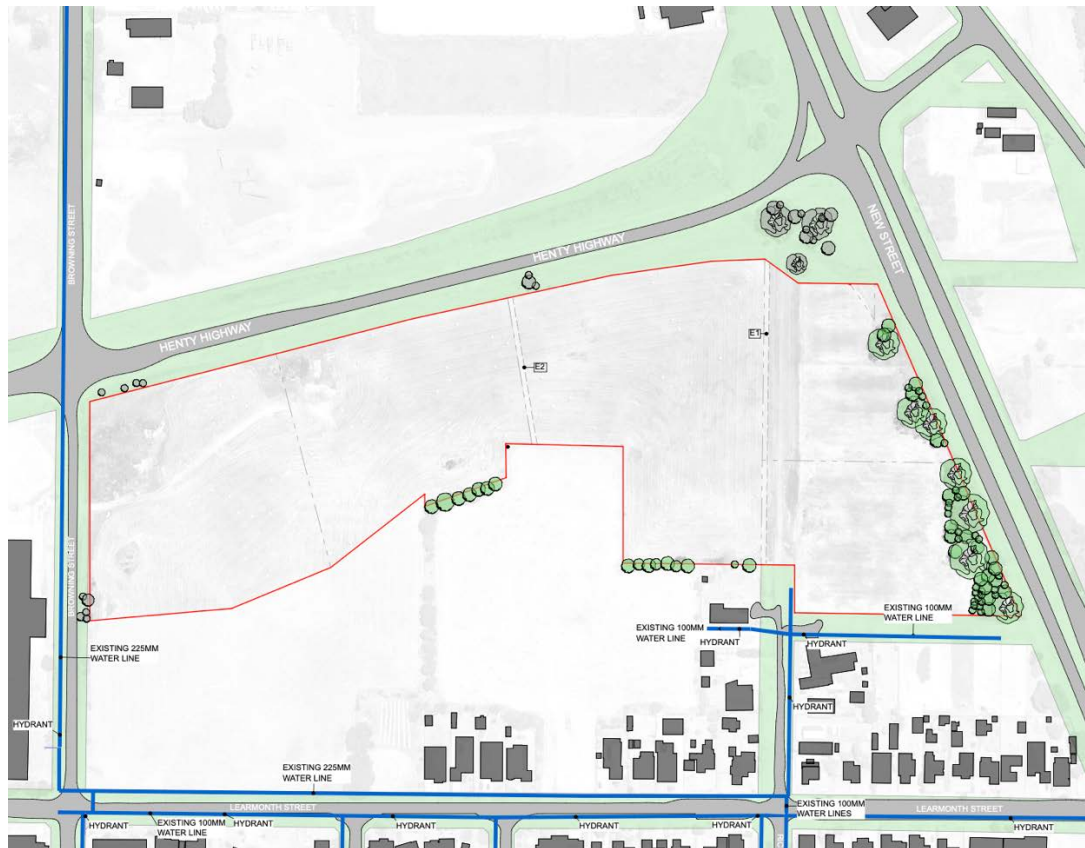


Figure 15: Water main asset detail (Wannon Water)

3.8 Electricity Supply

Powercor is the responsible service provider for the distribution of electricity assets in the region. There is limited existing infrastructure across all frontages of the subject site in Browning Street, Henty Highway and New Street. It is expected that new supply distribution substation will be required to any further development of the site. Current standards for residential and industrial developments require new underground power supply with pits installed on the property boundary with connection to each of the lots.

The construction of the power main to pits will be undertaken by Powercor at the developer's expense. The project can be registered online, and the developer will be required to sign a contract with Powercor prior to Powercor undertaking any works.

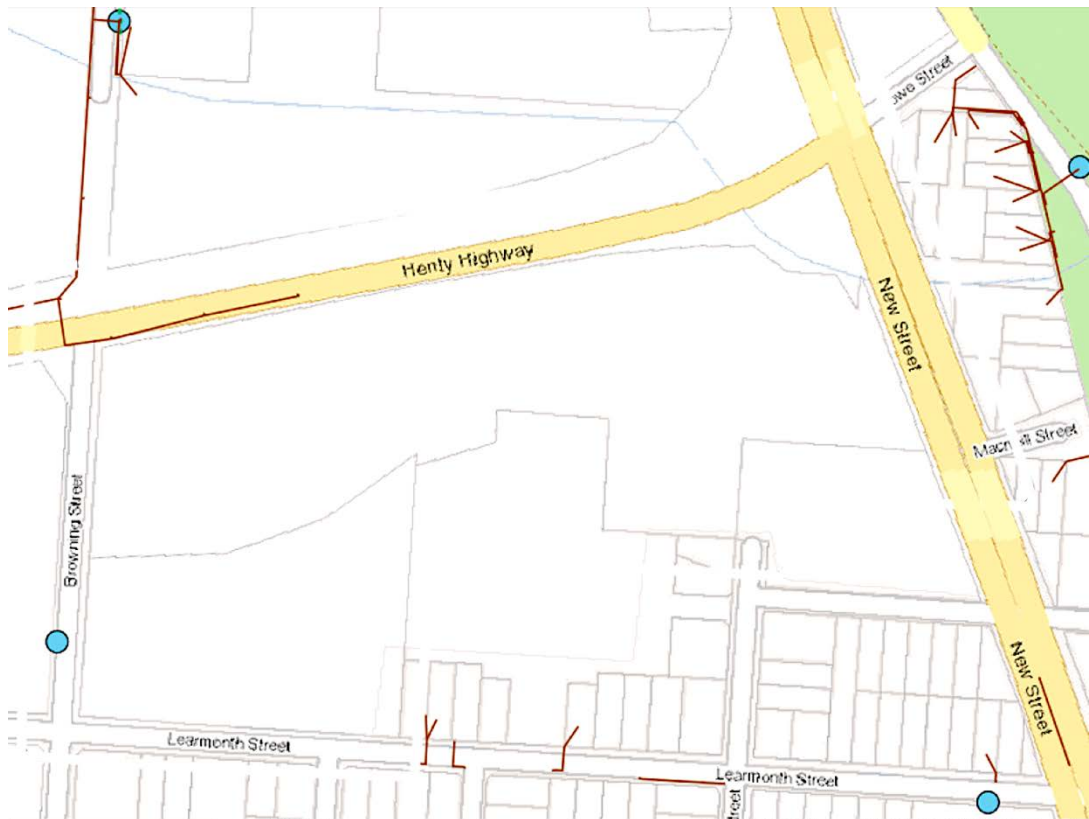


Figure 16: Power supply asset detail (PowerCor)

3.9 Gas Supply

Downer Utilities Australia Pty Ltd are the responsible authority for the provision of new gas infrastructure to the development. Gas Networks Victoria has provided existing gas infrastructure detail in the local area. There is existing gas infrastructure in Learmonth Street and in Richardson Street. Gas supply for any future proposed lots will require extension of this existing infrastructure. Refer to Figure 10 (overleaf) for more details.

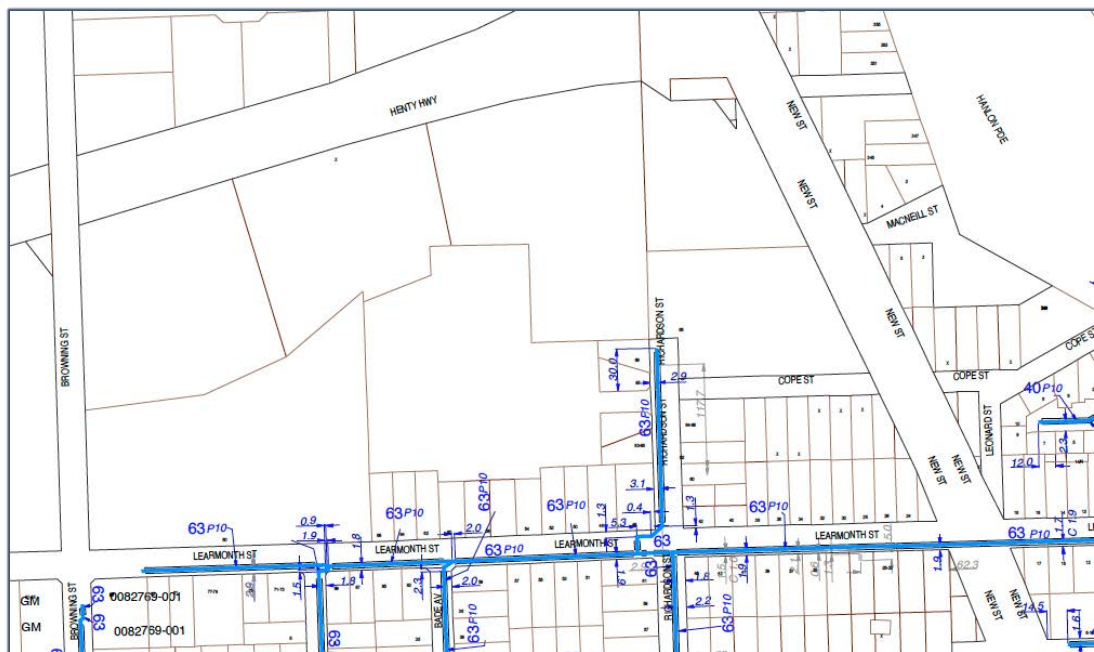


Figure 17: Gas supply detail – Blue, proposed line dashed Black (Downer Utilities Australia Pty Ltd)

3.10 Telecommunications

NBN Co. and Telstra are the responsible authorities for the provision of telecommunications facilities to all new developments in the area. NBN Co have provided locations of existing telecommunication cables and pits located in Learmonth Street, Richardson Street and further north traversing the site within existing telecommunication easements. Refer to Figure 13 for more details. Service connection of future development of the subject site will be required extension of this existing network to service all lots within the development area.

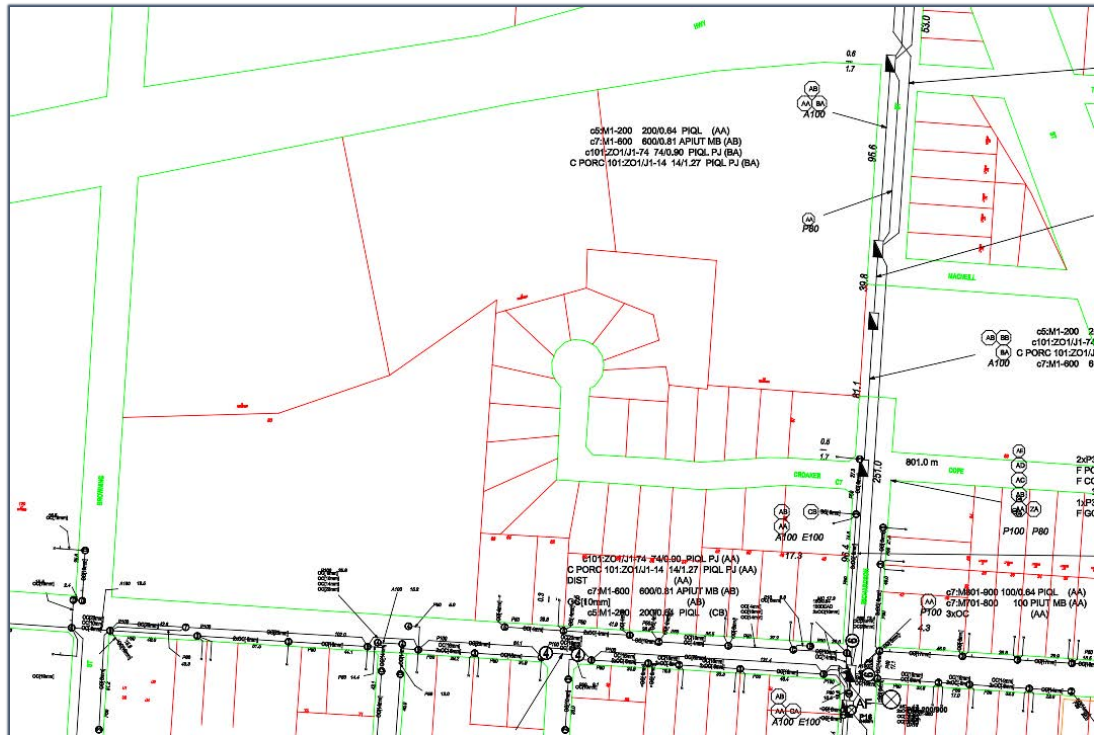


Figure 18: Telecommunications network (Telstra VICTAS)

4 Conclusion

This report has described and assessed the Site's existing transport connections and infrastructure, utility services, and the service utility implications to be considered as part of the Portland Gateway Precinct Development Plan.

Myers Planning & Associates

May 2023

Appendix C

Stakeholder Engagement

A draft version of the development plan was sent out for consultation with relevant stakeholders in early April 2023 for a period of two (2) weeks, ending 20th April 2023. Some stakeholders requested an extension of time to provide comment.

Glenelg Shire Council

Assets:

Comments were received throughout the preparation of the development plan with a focus on infrastructure services and engineering matters such as traffic and drainage.

These comments have been incorporated into the relevant sections of the development plan to ensure that the overall plan is consistent with the views of Glenelg Shire Council's Assets Unit.

Agencies

Department of Transport and Planning:

The Department of Transport and Planning provided the below comments as summarised via email dated 1 May 2023:

DTP has been working closely with the Glenelg Shire on this matter.

Both parties have arrived at an in principle agreement to the following key aspects to project regarding access to site from the Henty Highway.

Left in entry and exit movements only to the site from the Henty Highway (as per below concept design).

Please note that discussions are still being finalised regarding potential issues associated with Portland bound traffic from New Street and utilising the existing New Street – Henty Highway (Ring Road) intersection.

These comments from DTP enabled the development plan to nominate a location for a potential intersection to Henty Highway as well as an east-west connection from New Street through to Browning Street.

Wannon Water:

The following comments were received from Wannon Water via email on 28 April 2023:

Sewer

The area of interest shall be serviced by a new gravity sewerage network connecting to the existing Wannon Water gravity sewerage network to the northeast of the site.

A sewer at appropriate diameter, depth and grade to service this area of interest and the northern portion of Learmonth St subdivision will be required to be constructed under the Henty Hwy.

The existing gravity sewer is at significant depth so a gravity system connecting to the existing system can be designed to service the whole of the area of interest.

The 'pressure sewer' main that Myers have identified as a connection point is actually a sewage rising main from one of Wannon Water's sewage pump stations and shall not be used for servicing of this development.

Refer attached sketch.

Water

There is an existing DN225mm watermain running along Browning St. The development area should be serviced from new watermain extending from this main. Alignment of new mains will depend on development plan layout.

Watermain design for servicing this development area should avoid dead ends and include ring mains to avoid single point of supply. This may include connection to existing DN100mm watermain in Richardson St. A DN150mm main may also have to be extended up Richardson St from the DN225mm main on Learmonth St to service this industrial area depending on actual industrial development in this area.

These comments informed some minor text and mapping changes to the development plan, to ensure that a clear response was made that water supply to the development plan area would come from mains located to the west and that sewer would be serviced by the mains located to the north of the precinct.

AP Ausnet [Downer]:

No response received during engagement period.

Powercor:

No response received during engagement period.

Community

Landowner:

The landowner has liaised other parties intending to develop part of the site for input to draft development plan, in particular how application requirements and guidelines may impact a development application.

A summary table of requested changes were provided to Council on 21 April 2023

These comments have been incorporated where appropriate into the development plan.